

# THE CLASSIC SIX METRE NEWSLETTER

; DLCJ @ ECCK <

Number 13

Founded in 1993 by Tim Street

March 2008

[www.classic6mr.info](http://www.classic6mr.info)

Editors: Basil Carmody & Fredrich Dahlman

[basil@carmody.ws](mailto:basil@carmody.ws)

## Table of Contents

Page

Vital statistics	2
In's and Out's	2
Message from the Chairman of the Classics Committee	3
A proposal for the Annual Meeting, <i>by Henrik Andersin</i>	4

## Features

The Russian L 6 class, <i>by Vadim Manukhin and Scott Rohrer</i>	5
A set of plans for every Six, <i>by Martin Rosenstedt and Tim Street</i>	7
Transporting a classic Six – my personal experience, <i>by Fredrich Dahlman</i>	9

## Technical

Re-enforcing a wooden hull to resist the stresses transmitted by modern sails, <i>by Theo Rye, Kimo Mackey, and Brian Pope</i>	11
---	----

## Biographies

Architects: Arbaut & Arbeaut, <i>by Noël Gruet</i>	16
Builder: Henry Nevins	17
Sailmaker: Raimondo Panario, <i>by Jacques Dumon</i>	18

## Miscellany

Rescue Opportunity of 2008 and candidates for the years ahead	20
Kyla restoration needs sails	22
Flash – September metric regatta at Cannes	23
North American Web site	23

## News from the Fleets

24

1. *Agnes IV*, 2. *Aida*, 3. *Ayesha I*, 4. *Azaïs*, 5. *Bambi*, 6. *Belanea*, 7. *Blodoks*, 8. *Cailin*, 9. *Caprice*, 10. *Caresse*, 11. *Carin II*, 12. *Challenge*, 13. *Cherokee*, 14. *Ciocca II*, 15. *Clarity*, 16. *Clyde*, 17. *Clymène II*, 18. *Colibri*, 19. *Dana*, 20. *Djinn*, 21. *Edelweiss*, 22. *Elisabeth X*, 23. *Fiona*, 24. *Fissa*, 25. *Fokus III*, 26. *Goose*, 27. *Hakahala*, 28. *Houri*, 29. *Ian*, 30. *Jill*, 31. *Leila*, 32. *Lillevi*, 33. *Lisbeth V*, 34. *Llanoria*, 35. *Lucie II*, 36. *Lulu*, 37. *Madcap*, 38. *Maida*, 39. *Mandragore*, 40. *Mariana*, 41. *Melita*, 42. *Mena*, 43. *Miranda*, 44. *Mood*, 45. *Noreg III*, 46. *Normand III*, 47. *Oslo*, 48. *Renata*, 49. *Saleema*, 50. *Selma*, 51. *Silène III*, 52. *Sprig*, 53. *St Francis V*, 54. *St. Kitts III*, 56. *Steam*, 57. *Stella Polaris*, 58. *Sun Ray*, 59. *Tara*, 60. *Totem*, 61. *Vega*, 62. *Véga*, 63. *Venger*, 64. *Vert Galant*, 65. *Vingt-Ans Après*, 66. *Yeoman II*, & 67. *Zephyr*

## Classic Sixes newly for sale

Ayesha	64
Catherine	67
Marianne	68
Starwagon	71

## Resources

Register of existing Classics	74
Register of existing Moderns	80

## Vital Statistics

	<u>2008 estimates</u>
Sixes built since 1907	1550
Moderns built (almost certain)	-101
Classic reproductions (certain)	<u>-3</u>
Classic Sixes built	1348
Disappeared in one way or another	<u>-1023</u>
Classics thought to exist today	325
Whereabouts unknown	-9
Out of the water	-25
Undergoing restoration	-33
Status unknown	-122
Sailing or racing	135

## In's and Out's\*

### **Lost & Found**

#### **Lost (none)**

#### **Found**

- Cupidon-Fou, a 1929 Camatte design currently at Cognac, France
- Gefion III, a 1911 Anker design currently at Dellach am Wörthersee in Austria
- Leila, the first Six by Fife (1912) currently at Crinan, the western mouth of the Crinan canal, Argyll& Bute, Scotland
- Namoussa, a 1937 Louis Bréguet design currently at Vannes, France
- Rahahu, a 1914 Morgan Giles design currently at Pörtsach am Wörthersee in Austria
- Scout, the sole survivor of four Sixes built in New Zealand in 1909
- Vega, built in 1960 in South Africa currently at Port Owen, Capetown

### **Restorations & Replicas**

#### **Started**

- Cherokee: a "spec" replica of Olin Stephens' 1930 design, commissioned by the Museum of Yachting and being built by the International Yacht Restoration School at Newport, RI (USA), i.e. the boat is for sale and being built at the same time.
- Gulldisken: rearrangement of deck at the Guip boatyard in Brittany under the guidance of the naval architect Guy Ribadeau-Dumas
- Jill: a complete re-build of Olin Stephens' 1931 design, starting from the original ballast keel, by Rockport Marine, ME. (USA)
- Llanoria (*see News*)
- Madcap: refastening and a new deck for F. Hoyt's 1924 design, by the International Yacht Restoration School at Newport, R.I. (USA)
- Sonoma
- Totem: a major re-build of Billy Luders' 1930 design, by the Concordia Co, South Dartmouth, MA with help from Guck Inc. of Bristol, RI (USA)

- Zephyr: new frames and planking by Rexwinkel Jachtbouw in Zwijndrecht (Netherlands)

### Ended

- Agnès IV, after 10-15 years out of the water (*see News*)
- Fiona (*see News*)
- Fokus III, after 23 years out of the water (*see News*)
- Jill, (*see News*)
- Lulu, after 4 years out of the water (*see News*)
- Zephyr, (*see News*)

### Sales

#### Newly for sale

- Ayesha, **see listing at end**
- Marianne, **see listing at end**
- Mizar III, **see** [http://www.cantierisangermani.com/sito/ing\\_set.htm](http://www.cantierisangermani.com/sito/ing_set.htm).
- Star Wagon, **see listing at end**
- Véga, **see listing at end**
- **see also** 1.) [http://www.6metre.ch/market\\_place.html](http://www.6metre.ch/market_place.html),  
2.) <http://www.6mrnorthamerica.com/>  
3.) <http://www.metreyacht.com>

#### B. Recently purchased

- Gulldisken by Valentin Autret, France
- Lady Day by Erik Larsen, Denmark
- Mariana by Team Mariana, Finland
- Mosquito by Stig Hvinden, Norway
- Silvervingen by Fredrik Lindqvist, Sweden
- Sioma II, by Fenton Burgin, U.K.
- Star VI: by Ing. Christof Goritschnigg, Austria ([www.holzbootdokter.com](http://www.holzbootdokter.com))

### Name Changes

Artemis, ex-Mirage  
Erin, ex-Saskia II  
Oberspree, ex-Hunding VII  
Vi-Vi, ex-Hanko II

\* since the last issue

### Message from the Chairman of the Classics Committee

Busy times in the sheds – worldwide interest in Vintage Sixes restoration is increasing

No wrecks are safe – soon most of the Classic and Vintage Sixes will have been restored at least once. Much activity is seen in the United States, where the West Coast Sixes continue to be as active as the North European Fleet. I had a chance to visit Seattle and its surroundings in late 2007 and saw the busy shipwrights working on several boats, all getting ready for the Worlds in 2009. The New England fleet has suddenly become very active as well, sporting at least five restoration and reproduction efforts at the same time. We see activity on this front also in Europe, where the UK now has the biggest fleet of Sixes anywhere.

A general observation on the boats involved makes it quite clear that the new trends involved are *Originality*, *Second Rule* proper restoration and *Vintage interest*.

By *Originality*, I mean proper building and construction techniques and proper material use. We have come a long way – it is not coherent with a proper historic restoration to use glass

fiber, sheeting the hull with boards or impregnating the hull with epoxy. Epoxy is the glue of choice for everyone now, that's ok, but drowning the yacht in epoxy is not ok. I can see a thin 1mm outer epoxy paint being ok but I strongly advise you not to over do it.

Changing *Second Rule* boats to deeper Third Rule keels risks losing an important piece of history of the yacht concerned. It has not been done very much so far; but some fast supposed-to-be Second Rule boats now sport a Third Rule computer-optimised keel. They all measure in to the rule – but that is not the point.

*Vintage* interest is spreading to the United States, where Sparkman & Stephens “pancake” winches now are being reproduced in the Pacific Northwest. Wooden masts are showing up everywhere – with modern glues it is suddenly more feasible to build a proper vintage wooden mast again. They will cost 20.000 € each in full racing specification compared to an approximately 10-15.000 € aluminum version. Going Vintage is and will be more expensive than going Classic (by Classic, I consider the pre 1965 racing machines we see on the circuit where the main objective is to win). The hardware is harder to come by and it is often made of bronze or aluminum which don't last as long as the carbon fiber hardware available in all marine stores. The reason for going Vintage is because of the looks of the yacht. A well built Vintage Six does not lose to a Classic in racing speed.

Going Vintage includes horizontal-cut Dacron Sails. They look right, they are cheaper, but they do not last many seasons. A middle of the road approach here would be to have the main of Dacron (you should get 5 good years of sailing with it, or more) and the jib of something lighter. A thorough Vintage effort with wooden mast would go for all Dacron sets.

Henrik Andersin

## **A proposal for the 2008 AGM**

### **Carbon fiber Spinnaker poles should be banned from pre 1965 Six Metres**

I have noticed that in the last two years Carbon Fiber Spinnaker poles have made their way into Classic Sixes racing. I think this is a mistake.

Rule 33 states

*“Equipment, fittings, mast, boom and rigging on all yachts laid down after 1st March, 1972, shall be made of materials having properties not in excess of those of aluminum and its alloys and steel and its alloys, as given in the following table: ---*

AND

*Composite materials, such as those incorporating fibers of graphite, boron and other materials of limited availability are prohibited.*

AND

*Spinnaker Poles and their associated fittings may be made of any material.”*

This (or the M27 rule on Spinnaker Poles) should be amended in the next AGM for Classics to state that we prohibit the use of Carbon fiber in Spinnaker Poles on pre 1965 boats. If this amendment is approved by the ISMA AGM, according to the rule-change Rule yachts can still race with them in the EC in France 2008, but not in Newport 2009.

*Henrik Andersin is the owner of US 80 Djinn (Vintage) and FIN 51 May be VI (Classic)*

## The Russian L-6 class

Just at the time that the “classic” era of Sixes was ending, a new class of wooden yachts, the L-6, was born in Russia. The first boat was launched in 1963, coincident with the launch of the last classic Six, *AUT 47 Stromer*, designed by Olin Stephens. The L-6 is by no means a copy of Sixes, but one can sense the influence of the International Rule.

The origins of the class go back to the period 1932 - 1935 when N. Lüdwig developed the Russian Rule of Yacht Classification (RRYC). This rule gave rise to classes such as the L-30, the L-45, the L-60, and the L-100. (The International Rule 5.5 class was also active in Russia during this period.) The L-60 was designed by N. Alexandrov and Anatoli Kiseljov. About twenty were built during 1936 - 1941. It is considered to be the slightly smaller ancestor of the L-6. L-60's were named after Russia's rivers, e.g. *Svir, Igora, Volga, Luga* and *Narova*. Additional information on the RRYC can be found in the article “Yacht Rating” by Daniel D. Strohmeier (Marine Technology, April 1974, pp.125-133).



Anatoli Petrovich Kiseljov

At the time that the RRYC was revised in 1958, Kiselov, now based at the Leningrad Experimental Shipyard, designed the L-6. It won the competition for a new national class against another design, the CR-8 designed by N.V. Grigorie.

Kiselov died in 1962, just before the launching of *Angara*, the first L-6.

Russian companies bought the L-6's as part of company organised sporting activities. Others were purchased by yacht clubs. The in Germany crews ranged from workers and students to engineers.

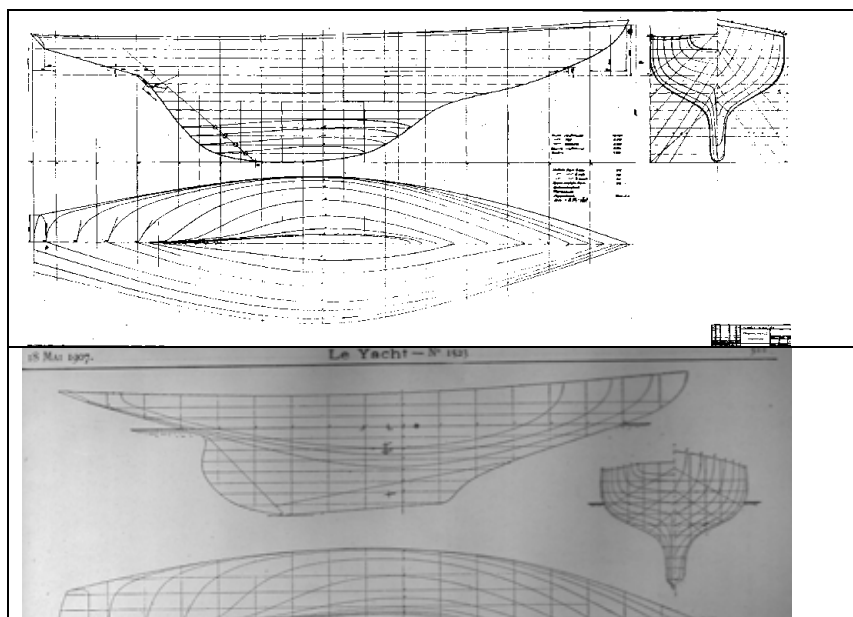
Interestingly, there is a project underway to build a modern version of the L-6, called the SOL-6 (see [www.sol-6.de](http://www.sol-6.de)).

As for the rating formula, the L-6 rating formula (left) is more inspired by the American Universal Rule (right) (where D = displacement) than by the International Rule.

$$16 = L + \frac{S}{5\sqrt[3]{D}} \qquad R = \frac{0.2 \cdot L \cdot \sqrt{S}}{\sqrt[3]{D}}$$

It is unlikely that the length L is measured identically in the two formulas. The major differences with respect to a Six are that the L-6 is a racing-cruiser, its length is slightly over 12 m. and its weight is 6.500 kg, with a ballast keel of 3.500 kg.

The two plans below are those of the L-6 (top) and a 1907 never-built 6mR plan given the name Arzel by the French architect Bosquain. There is a remarkable continuity of lines over 50 years of design.



The first photo below is of the launch of *Angara* in 1963, the two others are recent photos of *Nika*.



The L-6 is built with pine planks on oak frames with a cast iron ballast keel. Since there was no market for marine hardware, all the hardware had to be custom made. The first generation of boats had no motor.

Readers of Russian can consult the very extensive L-6 Web site at <http://www.nika-16.narod.ru>.

From 1963 through 1978, more than hundred L-6, and its modernised offshoot, the L-6m were built

Until the collapse of the USSR, L-6's were practically the only class of domestic yachts to participate in the largest USSR international competition of racing-cruising yachts, the Baltic Sea Cup, from 1971 to 1988. Contrary to Western stereotypes, the vast majority of Russians had no desire to leave their country. The races courses, which were 1000 miles long, could thus be



drawn close to the Finnish coast. In the entire history of the Baltic Sea Cup races during the Soviet period, there was only one case of a (failed) attempt to get to Finland.

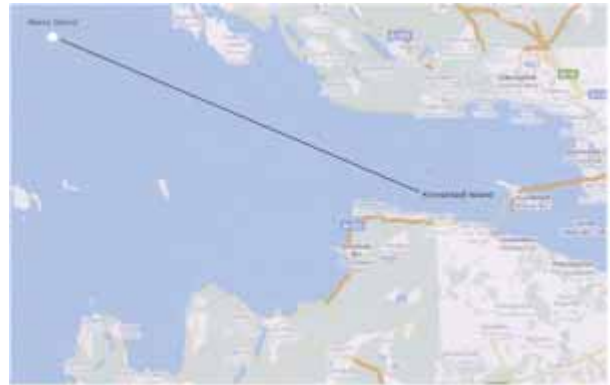
Being a national class, they became the training vessel for a generation of USSR

skippers. There are more than forty L-6's at Saint Petersburg of which half are currently in sailing condition. Other L-6's are in the Baltic countries, the Ukraine (Black Sea) and as far away as the Far East of Russia (the Sea of Japan).

Founded in 2001, the L-6 national association has revived the more than 150 year-old tradition of the 100 Mile Race. It is said that, in 1852, six British yachtsmen, smarting after the *America* debacle, set sail with their six yachts to wreak revenge on another country. They arrived at Saint-Petersburg just in time to participate in the newly instituted annual 100 Mile Race from Kronshtadt Island around Nerva Island and back. On 18<sup>th</sup> and 19<sup>th</sup> June 1852, the British yacht "Claimor" won the race in 14 hrs 48 min - which still stands as the record.



Course of the 100 Mile Race



**Vadim Manukhin** is the owner of the L-6, *Nika (Victory)*, president of the Russian L-6 Association and a professor at the Marine Technical University. **J. Scott Rohrer** is a former owner of Sixes and curator of the Seattle Center for Wooden Boats' 40 ft. loa Universal Rule R-boat *Pirate*.

## **A set of plans for every Six (Part II)**

In last year's letter, we mentioned sources of plans for the architects Camatte, Fife, McGruer and Mylne – and also the plans archived by Norske Veritas for the Scandinavian Sixes it surveyed during their construction.

We can add David Boyd's drawings which are held by The Archivist, Argyll & Bute Council, Kilmory, Lochgilpead, Scotland, PA31 8RT and the Camper & Nicholson ones at The National Maritime Museum, Woolwich. Fife's plans including Six-Metres are now almost all held by Fairlie Restorations, Unit 4, Port Hamble, Hants SO31 4NN, England ([duncan@fairlierestorations.com](mailto:duncan@fairlierestorations.com)).

We have also found an additional archive of Camatte plans near Bordeaux and we have been notified of the new ownership of the Mylne archives by Ace Marine at Fife.

The National Naval Museum of Oslo has plans for many of Johan Anker's boats. In the 2003 issue of this letter, Tim Street mentioned that Knud Reimers' plans for FIN 65 *Bambi* had been found at the Maritime Museum in Stockholm and that Christian Jensen's plans for N 71 *Flapper* were thought to be in the municipal museum of Soon, Norway.

There remain, however, many Sixes whose plans are permanently lost.

Until recently, the only solution for these boats was to "take off" their lines manually and to ask a naval architect to produce a set of "as is" plans.

One of the editors has done this for his Six. It took two novices about four days to produce the three sets of lines: waterlines, buttocks and diagonals. When a sample of the measurements was verified, the biggest errors were on the order of  $\pm 3$  mm. It then took an additional week to enter the data in the format required by the Delftship modeling software used by naval architects. One lesson to be learned from this is that one should know ahead of time the formats required by the computer program which will process the offset data. The original data should be entered into the computer with these final formats in mind.

There is also now the possibility of using laser-based technology which is faster and more accurate than manually measuring the offsets. It may also be cheaper in the end.

At least three Sixes have already used this technique.

- FIN 2 *Caresse*, a 1922 Zake Westin design, rebuilt by M-Yachts at Helsinki in 2000
- GBR 3 *Houri*, a 1911 A.E. Payne design, under restoration at Usk, Monmouthshire, U.K.
- USA 51 *Totem*, a 1930 A.E. Luders design, under restoration at Westport, Mass., USA

The story of *Caresse*, recounted by Martin Rosenstedt, follows:

The yacht *Caresse* was designed 1922 to compete in the fairly young class of 6mR yachts in Finland. Zake Westin, her designer, was later famous for stunningly beautiful designs such as *Merenneito I & II* and *Stella Polaris*. *Caresse* was his first six meter yacht. The yard chosen for building was Åbo Båtvarf, one of the best yards in this country with a long line of finely crafted yachts. We have worked on many of them, my personal favourite being "*Sphinx*" a 1928 Eight by Gustav Estlander.

*Caresse* was built on an oak backbone and galvanized frames. She had a short racing career in the early 1920's, and we never found out whether she originally had a cabin or if it was added after one or two seasons. She was used for cruising for many decades.

In the 1990's, she competed in a classic regatta in Sweden, so her owner claimed she must be in good condition! When the yacht arrived at the yard for a complete restoration, we of course tried to make a plan of how to save as much of the original boat as possible.

Very soon we unfortunately discovered that she needed to be rebuilt as a replica; the backbone was unmeasurable, the planking worn down to 10mm and so on. The only parts of her hull that were in reusable condition were some of the steel frames. As it turned out we were able to use just five complete pairs in the finished yacht. Original fittings; rudder, deck and rigging were of course reused.

In order to get the material for lofting her, we had only two alternatives, due to her badly deteriorated backbone (she was severely twisted and hogged): cutting the boat to pieces or using digital measurement techniques. As it finally turned out, we did both.



The digital measurements were carried out on the better side of the hull (she had fallen off her cradle) and the idea was to create drawings usable instead for conventional lofting which is time consuming and hard work on your knees.

We used available techniques at that time (1999). A grid was made by putting timber at a straight line parallel to the centreline. She was resting on a gravel floor which posed



considerable problems during the whole measurement process. I resolved that if I ever were to use this technique again, I would use a perfectly level concrete floor.

The data was sent to a naval architect to be processed. It later turned out that his experience was limited to designing cruising ships. When we received plots of the frames, they did not match the old ones, so we took the difficult decision to cut the boat to pieces. This was, in fact, the only way to remove the steel frames out without losing their shape.

Our lofting was then a combination of old and new; digital data and old rusty frames on the floor.

The technique has improved since then. Working with an architect with experience with Sixes, this CAN be the way to go in the future.

Several years later, we did a 55 m<sup>2</sup> skerry cruiser using laser measuring. It provided us with usable printouts, and no lofting was required.

\* \* \* \* \*

Given the value of the heritage which the current fleet of classic Sixes represents, the editors encourage all owners of Sixes without plans to add a laser modelling to their maintenance budgets for the next few years. This would avoid the total disappearance of an existing boat due to the rare, but catastrophic, ravages of fire and other accidents.

*Martin Rosenstedt* built his first boat at the age of 15 and, since then, he has never understood that the possibility of stopping existed. Now he is running a boatyard with a crew of five and a big number of old woodies. He currently sails a 1948 Tore Holm Baltic Eight, and can be seen in the crew of almost any classic yacht *Tim Street* is owner of the Six, *Abu*, co-author, with Pekka Barck, of the book *The Six Metre – 100 years of Racing*. and president of ISMA.

## **Transporting a classic Six – my personal experience**

### 1. Lifting

Transport itself is usually not the most dangerous part, but rather the lifting of the boat in/out of the water and onto the transport.

The most frequent cases of damage to boats have been caused by crane personnel. The struggles with crane operators to lift our boats correctly have been numerous and sometimes tempestuous. The operators always think that they know how to lift a Six and are offended if told how it should be done. Never hesitate. Supervise it yourself!

The lifting of classics with two straps, with the aft one passing under the counter above the rudder, is a disaster - especially for older boats which haven't been refastened.

At the WC in Saint-Tropez in 2003, *Alexandra* was damaged by improper lifting of this sort. The stresses applied to *Alexandra* led to the opening of a plank seam which required lifting her out again almost immediately and the intervention of a marine carpenter to close the seam. She also missed several days of participation because of this emergency.

Similarly, at the 2007 WC at Cowes, we observed damage inflicted to *Fléau*, the World Champion modern Six, due to wrong hoisting at Hamble. Again, straps had been improperly passed around her outer body.

We always lift *Fågel Blå* with one strap under the front part of the keel and the other strap under the aft part, forward of the rudder. We always insist on the straps being adjusted so that she is lifted in horizontal position. It is also very important to connect the forward and aft lifting straps with two (tight) diagonals to prevent the lifting straps from moving during lifting. Do not compromise on these points!

The alternative to using lifting straps is to lift the boat by an internal hoisting ring connected to one or more keel bolts inside the boat. This is not suitable for every boat. If you don't know the condition of your keel bolts, it can spell disaster. Many boats that regularly used

this method, but without verifying the condition of the bolts and rings each time, have fallen during lifting when one of the components gave way. If, however, the keel bolts have recently been renovated, this is by far the best method.

## 2. Transporting / Transit

The safest solution is to own a dedicated custom made transport vehicle. Two other solutions exist: 1.) a general purpose truck or trailer adaptable to Sixes (see IYIM photos below) and 2.) the use of a general purpose low-deck flat bed lorry with the boat on a cradle – or the variant of placing the boat and cradle in a maritime container, which is the usual solution if long-distance maritime transport is involved.

There are several importance issues for this second case. The most important one is the need to fix the keel securely to prevent it to move around in the cradle. Make sure you have a deep enough trough into which the keel can fit – or alternatively use a custom-made form bolted to the cradle. It is also very important to ensure that the cradle including the boat cannot move forward on the lorry floor. This can be achieved by first fixing the cradle, without the boat, onto the lorry floor. A heavy duty wooden or metal pole can be used to prevent the cradle from moving forward.

The next issue is to prevent the boat from moving once it is on the cradle. The most common mistake in this



International Metre Yacht Management's trailer (with *Ciocca II*) and truck (with *Caprice*)

regard is to lash the boat down too strongly with straps. This can lead to breaking the hull. For *Fågel Blå*, I use 12 straps in all - 6 forward and 6 aft, lightly tightened. This approach keeps the boat in its position without putting stress/pressure on the hull. Putting plastic or rubber dock bumpers under the straps avoids nasty marks on the hull. (See below in the article “Rescue Opportunity for 2008 and candidates for the years ahead”, the case of US 64 *Tao* that suffered multiple broken frames during truck transport across the U.S.)

The last thing to keep in mind is the construction of the cradle. Metal cradles are always the best. It always pays off to also run straps between all the legs of the cradle. If the boat moves and puts too much pressure on one leg, the straps will transfer some of the pressure onto the other legs. When *Delphina* was being shipped to Saint-Tropez in 2003, there was a chain reaction which started because the keel was not sufficiently blocked in place. This led to the hull shifting in the cradle which, in turn, resulted in too much pressure being applied to one leg of the cradle. The leg broke under the strain, causing damage to *Delphina's* hull, as she fell on the trailer.

Shipping *Fågel Blå* to other European destinations necessarily involves one maritime leg, thus all the dangers caused by the pitching and rolling of a ship. I hope that the lessons that we have learned from this can be useful for the owners planning to ship their Sixes Newport next year.

(See also the recent article “Moving Experience” in *ClassicBoat*, no. 238, April 2008, pp. 30 – 35)

**Fredrich Dahlman**, is co-editor of this newsletter and World Champion for classic Sixes (Cowes 2007).

## Reinforcing a wooden hull to resist the stresses of modern sails

### Reinforcing a wooden 6mR hull, by Theo Rye

Wooden 6 metre hulls, especially those with long overhangs, present an interesting set of design issues. One of the most common is how to reinforce the hull to resist the higher loads of a modern rig.

As long as we were happy to stick to the old way of things, with spruce masts and “soft” sails, everything was fine. The problems started with alloy masts; then rod rigging, and laminate sails; in order to get these things to work well, the rigging had to be set up tighter and tighter. It worked, but many of the older carvel hulls fitted with modern rigs were soon showing signs of hard usage; splines or seams cracking and opening, garboards leaking, decks hogging.

The first thing to understand is that, generally, traditional yachts are pretty good at resisting the loads they were designed for: they keep the seawater out (from below, at any rate), and rarely break. Occasionally very old yachts do “sag” at the ends a bit; but generally that’s after a long hard life. Put a modern rig in though, and wind it up a bit, and things soon change.

Broadly speaking you can break the sources of these loads down in ascending order of concern;

**Standing backstay:** rarely a major issue despite being right at the aft end of the overhang, as they are on the centreline, and not normally highly tensioned.

**Headsail sheets:** usually only a cause of local problems as the loads are generally pretty flat to the deck.

**Halyards:** Despite being so close to the critically loaded area of the mast, halyard loads are normally pretty easy to deal with by sensible local design.

**Mainsheet:** Often a big load, but quite close to the centreline and again more often a source of local concerns (track pulling out) as it is usually far enough into the main body of the boat not to cause global bending issues.

**Headstay:** This has to be taut for effective windward work, and the result is inevitably high loads on the centreline, often out at the end of a fine overhanging bow. On many older yachts the situation is even worse as the forestay is pushed forward from the original position in order to increase the J measurement.

**Shrouds:** Shroud loadings are now typically high even at rest. They are pulling up and inboard, causing local deck compression transversely at the mast, and sometimes “hogging” the hull longitudinally as well.

**Running backstays:** Usually the worst of the lot. In order to get good headstay tension, runners are placed as far aft as possible, and set up very tight; and the loads are cyclical and variable. Being offset from the centreline, they introduce a very nasty torsional (twisting) load in the hull which traditionally built carvel hulls are not good at resisting.

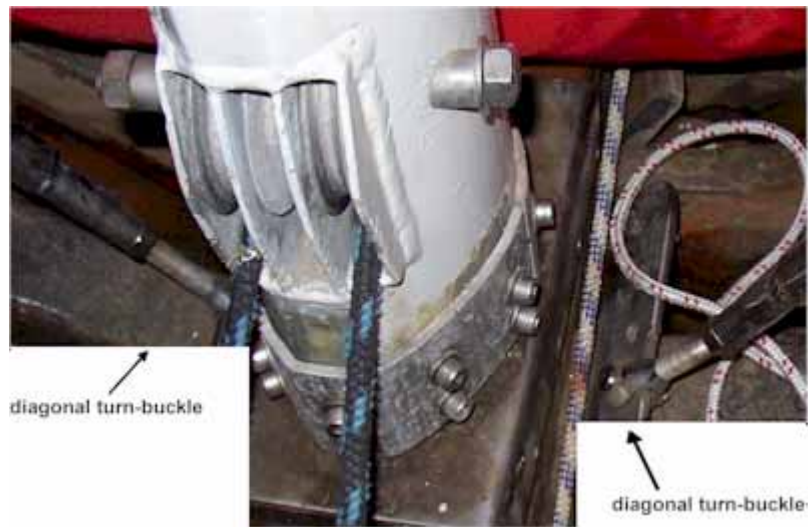
Ultimately all these loads are the result of trying to keep the mast in the boat and reasonably straight. The mast itself is then pushing downwards on the step, generating big compressive loads.

So how to work through that lot?

The first thing to bear in mind is that a reasonably sound hull is the only place to start. If the hull is old and tired, no amount of secondary reinforcement is likely to work. A well built wooden hull, with sound plank and hull fastenings and exhibiting no abnormal tendency to work or flex, is the only worthwhile starting point. The clearest indication of a wooden hull “working” is usually plank seams showing. On a well found hull it may be possible to stiffen the hull generally by the traditional method of splining the seams (inserting timber fillets, glued along both edges to the neighbouring planks). This helps prevent the differential movement along the plank edges. If that doesn’t work or isn’t an option, we need to go into more detail about the causes and potential solutions.

Let’s start with the global loads. Almost all sailing boats are resisting loads which try to fold them up in the middle. The forestay and backstay / runners are all pulling upwards, whilst the mast pushes down. So we have to try and ensure the hull can resist that. All first-year naval architects are introduced to the concept of the “ship girder”; they are taught to treat the hull as a box; the deeper the box, and the stronger the bottom, the better it will be at resisting the long bending loads. Fortunately a 6M is usually quite a good shape box, being relatively deep; this is especially fortunate because adding material to resist these longitudinal loads is difficult once the boat has been built. In a wooden boat, it is usually impossible to effectively or efficiently add material low down where it is most helpful (near or under the wood keel or ballast keel), so we are normally looking for ways to support and brace the ends; pillars and braces for the overhangs, and reinforcing the stem to wood keel or counter to sternpost connections.

We also need to make sure the forestay load is transmitted into the only substantial structural item around; the stem. That can mean taking the end of the forestay down through the deck and actually attaching it directly to the stem face, or more commonly having a tie or wire brace from the top of the inside of the stem to the underside of the head-stay fitting.



Above: bottom of the diagonal assembly – mast step with turnbuckles connected to the bottom of the connecting plates (*Photo of Bihannic, taken with the permission of Laurent Lafaille*)

Left: top of the diagonal assembly – metal plate on the beam shelf tied into the shrouds and then connected to turnbuckles and connecting plates (*Photo of Bob Kat II, taken with the permission of Doug Petersen*)

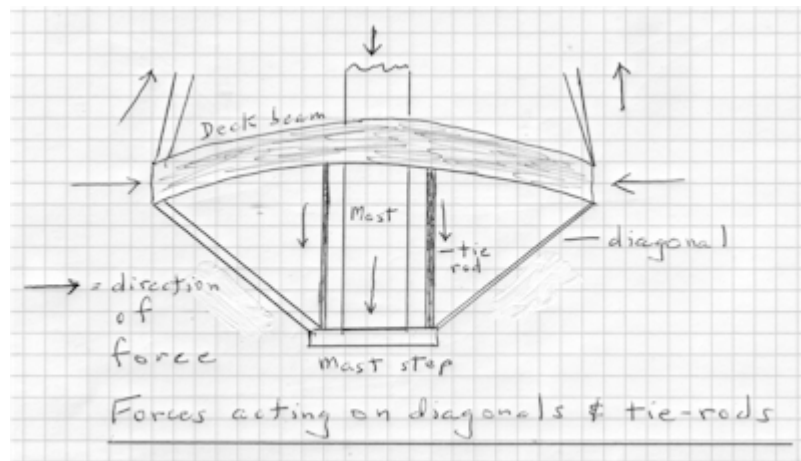
The transverse loads from the shrouds at the mast can be met in two ways. Firstly, we want to try and stop the deck flexing upwards by running braces to the mast step. These are usually rigging wires, rods or plates set from the underside of the chain-plates at the sheer strake down to the mast step (*the “diagonals” referred to in the photos*). The angle of these wires is usually not really ideal; you’d like to pull straight down if you could, but the strength of the mast step often makes it the only option. As these wires are often at close to 45 degrees, when tensioned they are then also trying to pull the sides of the deck together, making the local compression loads worse. So it is sensible to make sure the deck structure is adequate. The deck camber is working against you here, so reinforcement of deck beams is often coupled with a tie rod (*see the varying view of Brian Pope on this point below*).

A vertical tie rod or bar is a pretty traditional item, but it is surprising how often they are mis-used or removed altogether. A tie rod should ideally be a fixed length and as close to the mast as possible, normally just aft. Its primary function is to resist the deck from moving upwards; “shrugging” (the terms is by analogy to the movement of the shoulders either being raised or lowered, when shrugged) out of the way of the compression loads. If this rod can be tied into the transverse reinforcement of the deck beams, that’s even better.

And now, the really tricky one; the backstay runners. The first problem is their contribution to the global longitudinal bending moment. As discussed, this is difficult to resist by retrofitting material to an existing hull. If you are re-rigging, it is worth keeping in mind the balance between the mechanical advantage of runners set well aft against the practicality and lower loading of a position farther forward. As you move forwards, the increasing depth of the hull generally means it is better able to resist the loads; and conversely pushing them out into the counter gives the hull less of a chance. If possible, the runner loads need to be distributed into the longitudinal structures as effectively as possible; usually the beam shelf and sometimes the bilge stringer as well. That will help with the torsional load as well; again, retrofitting a solution to this is not easy. Ideally, diagonal braces or structure would support the hull, but that can only effectively be added during the original build or during a comprehensive rebuild.

Load distribution is also the key to the third problem, which occurs with all our rigging loads to some extent; local strength. Attaching a highly loaded wire to a wooden structure requires a reasonable knowledge of timber; the key being strength under compression. For example, it is usually sensible to let deck fittings such as cleats into the deck surface slightly. That way the shear strength of the bolts or screws is working with the compression strength of the edge of the timber. Similarly it is always worth trying to lead the serious loads to and through substantial timber items, and then distributing them with oversize plate washers.

Every boat is, of course different. Starting from scratch, for a new build or complete rebuild in wood with a modern rig, additional torsional, longitudinal and transverse strength can be designed and built in. That might be by employing a cold-moulded hull with diagonal veneers, (or even GRP cloth), set normally at +/-45 degrees.



There are cases of 6 Metres which have had additional hull reinforcement of this type applied over their existing carvel planking. This is a radical approach and will not suit everyone; it adds weight and volume to the hull and complicates any future repair work, and may reduce their value in the longer term. On the other hand it can be an effective way to deal with

serious wracking or torsional issues, or longitudinal bending, which are hard to address otherwise.

Additional or deeper internal structural elements to support and reinforce the existing may be a more benign option, and this approach is usually the most cost effective and efficient; although the issues that can be remedied like this are limited.

In summary, the loads of a modern rig can cause problems for a traditional wooden hull. Resisting them will require a balanced approach and a good understanding of the loads, and is likely to be a compromise between cost, degree of work, and effectiveness.

### Restoration practice at Ocean Yacht Co, by Brian Pope

We think that, having now done quite a few Six Metre restorations, *Toy* being the latest, we have found several ways to resist sail stresses. A point worth noting, *Titia*, restored by us, was the only boat at the 2007 Worlds not to break anything!!

We normally carry out the following modifications:

1. Running backstays: **These need to be as close to the centre line as possible. To avoid having too much line to winch in, the backstay is not rigged with a pulley block. The backstays are led under the deck and then re-fed back on deck to the winch farther forward.**

To accomplish this reversal of direction, we build a “runner box” situated under the aft deck, tied in to the horn timber. A small floor adjustment is normally required. Harken high load sheaves are used. They’re through bolted to both sides of the box. (They must be removable for regular maintenance.) The box is built with plywood at least 19 mm. thick.

Compared to having a rig with a pulley block, the stress on the winches is doubled. They must thus be sized accordingly

2. Mast sections and surrounding areas: **More complex but in essence quite simple**

**Beam Shelf:** We add a second beam shelf about 3 m. long, glued and screwed to the existing shelf and tapered at each end. This will help resist “banana” bending of the hull.

**Ring Bulkheads:** We install 2 bulkheads (ring frames) of 19 mm. plywood, shaped to the contours of the inside of the deck and hull.. These are fitted to the inside of the planking and/or frames as firmly as possible (screws, glue and bolts). One is forward of the mast and the other aft of it.

**Anchor tubes running between the bulkheads:** When the bulkheads are in place, we install two robust stainless steel tubes fore’n aft between them. The tubes serve as anchor points for 1.) the shrouds and lower stays which pass through holes in the deck and 2.) the diagonals which run to the mast step. In this way, the opposing forces of the rigging are concentrated on the tubes, which in turn transfer these forces to the ring bulkheads.

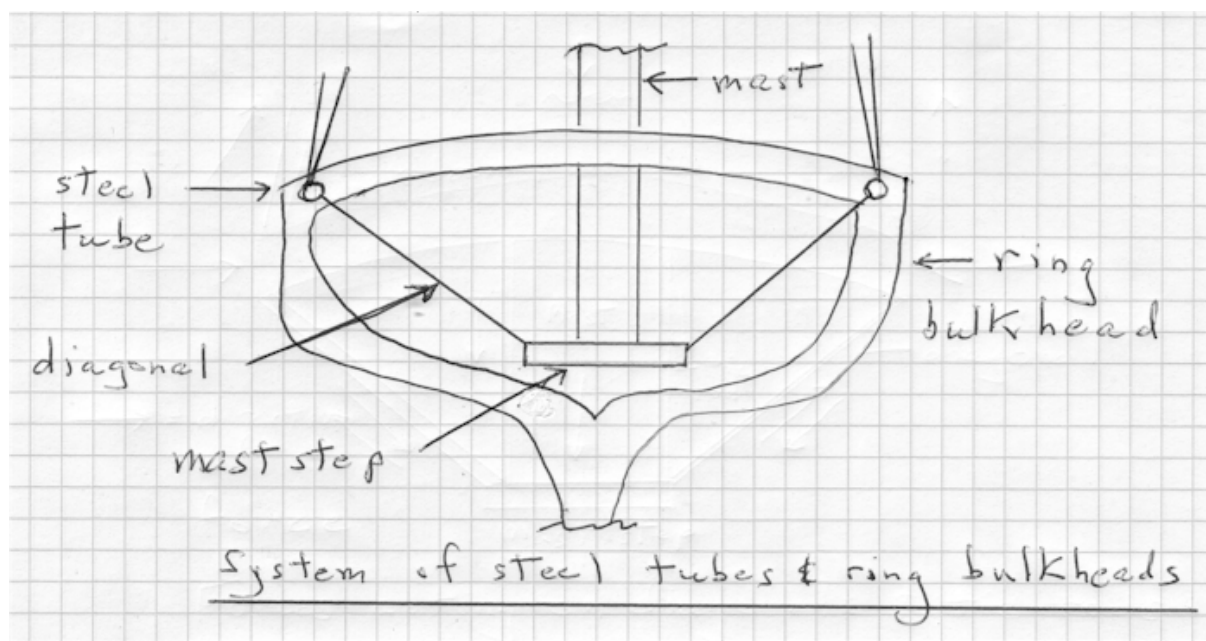
**Mast Step:** A stainless steel band under the mast step is required to take the force of the diagonals. This arrangement will create a complete triangular space frame, distributing the loads over a large area of the hull, thus allowing maximum rig tension. The lateral deck beam structure in this area must also be robust.

With these modifications, the keel no longer has to bear the tension of the rigging.

3. Bow section: **We favour a foil forestay with furler. Normally, we anchor the forestay to underside of the stem.**

**Stainless steel furler mounting:** It is important to ensure that it is fully installed before applying full loads to the running backstays. This approach avoids loading the deck and can be used even if the frames and the inner stem are not in perfect condition.

4. Decks and deck beams: **A plywood sub-deck greatly strengthens the deck. The original deck beams of wooden Sixes are not strong enough to withstand the loads imposed by modern sails. The camber of the deck increases its strength. Compared to a deck with a good camber, a flat deck will withstand less compression before collapsing.**



### Comments, by Kimo Mackey

I consider it worth stressing the very significant increase in hull strength which results from the topside planks being well glued to one another at the edges for the full length of the hull. Theo Rye very correctly points this out, but I wanted to emphasize how substantial is the contribution to longitudinal strength of the structure.

Brion Pope's "**plywood ring bulkhead and diagonal tie rod solution**" for opposing the athwartship shroud loads is without defect structurally, but some of us may find this solution less than ideal for application on a Classic 6mR ("plywood ring bulkhead?...I don't think so"). My objections to this solution are not only aesthetic but practical as well, because the arrangement hugely restricts access into the bow of the boat (about the only place to store all your clobber).

I used to believe that the **ring bulkhead – diagonal tie rod solution** was the only one which could adequately resist high shroud tensions, but I have recently had experience with a very good system done by Red Sky Craft (Kotka, Finland) on Henrik Andersin's *May Be VI* (and also on *Fridolin*). In this system the "chainplate" consists of a series of stainless steel straps (4-5 per side) which radiate far below the sheer strake/clamp and continue deep into the hull (all the way to the bilge in a fanning pattern). The straps are through fastened as they pass over each frame and they are also fastened to a stringer (not part of the original structure but added to increase strength in the way Theo has recommended). In this system the diagonal rods are not used so there is free unobstructed passage forward alongside the mast. As in the

alternative systems, the deck beams fore and aft of the mast must be very stout and tie rods to prevent shrugging should also be employed.

This is an excellent example of spreading the loads deep and wide into the structure of the boat. This is a fundamental principle that should be adhered to at all stress points (runners and head-stay as well).

I sailed on *May Be VI* in the 2005 WC. The shrouds were set up very tight. I inspected the hull closely to see if there were any signs of movement as a result of the high shroud tension. There was no evidence of localized stress (no cracking paint at seams inside or out even after several seasons of hard racing). I also checked *Fridolin* whose shrouds were VERY tight and she too showed no signs of movement. I intend to switch *Saga* over to this system when I renew the deck next winter. I am convinced that it works well if done correctly, allows free access into the bow of the boat and is more in keeping with the Classic 6mR aesthetic.

*Theo Rye*, a naval architect specialising in classic yachts, is a regular contributor to this newsletter. *Brian Pope*'s boatyard, Ocean Yacht Co. at Pen Pol in Cornwall, is the most active restorer of Sixes in the world. *Kimo Mackey*, owner of one of the two classic Sixes named *Saga*, is a member of the ISMA Classics Committee and donor of the Queen Christina Nations Cup.

### Architects : Arbaut and Arbeaut

The main reason for choosing Pierre Arbaut and François Arbeaut as the feature architects of this issue is to clarify the confusion related to the similar spelling of their names.

They were both French architects, whose Sixes overlap for a few years. Arbaut was based at Le Havre and Arbeaut in Paris. Right from the start in 1907, two Six Metre fleets formed in France, one on the Seine stretching from the Paris region to Le Havre and the other in the Arcachon basin (near Bordeaux). Seine and Arcachon region builders continued building Sixes up until WWII, gradually replaced by Mediterranean builders who continued building wooden Sixes up until 1952.

François Arbeaut and his Bordeaux counterpart, Joseph Guédon, dominated the first generation of French Sixes. Arbeaut already had four to his credit in 1907. In 1910, he became the first French architect to design for both the Seine and Arcachon fleets. In all, he designed 13 Sixes during the short period 1907 – 1913.



*Atout*, one of François Arbeaut's three 1907 Sixes (photo: *Le Yacht*, courtesy of the Musée National de la Marine, Paris)

In 1913, characteristic of the keen competition between architects, the French architect, J. Quernel, damned Arbeaut's elegant Six, *Sans Souci II*, with faint praise, saying:

“It's really a handsome Six. In fact, I sense the influence of G. U. Laws' *Sioma* and *Bunty* that Mr. Arbeaut would undoubtedly have observed during last year's races. *Sans Souci II* has harder chines and leaner lines than his previous boats”.

Furious at the suggestion of plagiarism, Arbeaut's pulls out all the stops in his reply:

“*Sans Souci II*'s design was no more influenced by *Sioma* or *Bunty*, products of my distinguished colleague Mr. G. U. Laws, who is seven



years my junior, than by the frigate *Belle-Poule* which, as you well know, had the honour to return the Emperor's ashes to France. [The repatriation of the ashes in 1840 is one of the defining moments of French history. It was experienced as a resurrection. The mention here of a frigate is a play on words since the French word for tumblehome is "frigated"]. I have always been an advocate of tumblehome for racing yachts. All the Sixes I've designed, beginning with Madame Poulet [1907], have had hard chines and lean lines."

Pierre Arbaut's first Six was launched in 1911 and he went on to design 19 in all, his last being Normand IV of 1930. All his Sixes were built by yards in the Seine region, mainly at Le Havre. Two of his Sixes exist today. They are OE 13 Freya which is thought to be in Switzerland and FRA 27 Normand III which will be competing in the 2008 European Championships. Arbaut's best known metre yacht is his Eight *Aile* with which Virginie Hériot won the 1928 Olympics.



Virginie Hériot's F 42 Petite-Aile III (1930), one of Pierre Arbaut's last Sixes (photo: *Le Yacht*, courtesy of the Musée National de la Marine, Paris)

*Noël Gruet* is editor of the *Cahiers du Bassin*, devoted to the maritime history of the Arcachon basin.

### **Builder: Henry B. Nevins (1878 – 1950)**

The principal builder of Sixes in the U.S., Henry Nevins built about forty of them between 1921 and 1948. He thus ranks 5<sup>th</sup> overall, only exceeded by the Europeans Anker & Jensen, Bjarn Aas, Abeking & Rasmussen and von Hacht.

His domination of the construction of U.S. Sixes is evidenced by fact that he built all but one each of C.S. Hoyt's and Clinton Crane's designs and all but two of Olin Stephens U.S. built designs.

Among the 19 Nevins-built Sixes that still exist today, one finds such illustrious boats as Bob Kat II, Clytie II, Djinn, Fun, Goose, Llanoria, Lucie II and Nancy. Others such as Jill and Madcap are being rebuilt or restored in preparation for next year's World Cup.

The Nevins yard was located on City Island, part of the Bronx, constituting the most northeasterly part of New York City. The island marks the end of Long Island Sound as it becomes Eastchester Bay, which then becomes the East River which borders Manhattan.

Henry Nevins was born in New York in 1878, and wanted to be a doctor but was too frail, so he decided to work at his hobby, shipbuilding. He apprenticed out to Charles L. Seabury, and at 29 bought the existing Hansen yard on City Island, renaming it Henry B. Nevins, Inc.

When the yard finally closed in 1954, Time magazine wrote:

"The late, great yachtbuilder Henry B. Nevins was never a man to cut corners. His City Island yard in New York City seasoned its own lumber, designed and machined its own fittings, fastened its spars together with glue made of sour cream, sometimes trimmed them to the correct balance by weighing shavings. By such attention to detail, Perfectionist Henry Nevins built more cup-winning yachts than anyone else.

As his fame grew, boatbuilders came from Scandinavia and Scotland to work for him. Nevins knew every employee by his first name. Even

after he became a millionaire, he often brought his own lunch pail to work, ate outside with the loftsmen and mechanics. His friendship and personal ability invited them to do their best work; his high standards demanded it. Once he set down this principle: "The man who builds . . . yachts is a craftsman; outside of yacht building, there are few craft industries left. A good craftsman must have, first of all, a basic sense of integrity and pride in his work . . . He is only secondarily materialistic."

Few owners ever asked Nevins for a price before signing the contract, relying on him to set a fair charge when the boat was delivered."

He built over 700 yachts, of which Sixes were possibly the smallest, and also 24 (wooden) minesweepers for the U.S. Navy during WWII.

### **Sailmaker: Raimondo Panario**

The story of the beginnings of the Genoa jib has been recounted several times, most recently by Tim Street and Pekka Barck in *The Six Metre*, but previous accounts have been based on peripheral sources, primarily Danish and American. The following account provides some new details and a glimpse at the modest personality of Raimondo Panario. Surprisingly, it was found in Italian on a Russian Web site, otherwise entirely written with Cyrillic characters – and the Russians had copied it from a Ukrainian site!

The definitive version undoubtedly appears in the book *Genoa Jib : cento anni di vela in Italia*, by Carlo Tagliafico and Tino Delfino (Genoa, Casa Editrice Marietti, c. 1988, 309 pages) which is out of print and unavailable.

The story begins during WWI in the town of Voltri, 20 km. to the west of Genoa, where Raimondo Panario was employed as a sailmaker and then turns to Turin where the Aeronatica Pomilio di Torino (APT) was building fighter planes. On these planes, taut wires ran between the pilots' control levers and the control surfaces of the wings and tail structure. The best specialists of this type of wire were the riggers of masts. Accordingly, APT had hired the Voltri team to come to work in Turin.

While at Turin, Panario longed for the war to end so that he could return to his native Genoa. He had learned the ways of the sea as a youth, accompanying his father on small coastal voyages on their boat, *Giraffa*, in the upper part of the Tyrrhenian Sea. Before the war, he had already won a handsome collection of medals after various campaigns of international regattas. He remembered with nostalgia the names of his boats which had given him the greatest satisfaction: *Calliope, Lina, Teresina, Titania, Cesarina, Ziska, Vampa, Sirdhana*.

Finally, peace returned and Raimondo Panario returned to sailmaking.

In addition to the challenge of reviving the yachting sector after the war years, architects, sailmakers and yachtsmen throughout Europe were preoccupied with changes required by the introduction of the Second International Rule in 1920. This gave rise to an intense period of experimentation by scientifically oriented designers. The new rule favoured a triangular mainsail and a so-called Marconi mast. It definitively eliminated gaffs and topsails, with the end result that sail handling became decidedly simpler.

There were thus new hull forms and new sail plans to discover. The race was on to produce new yachts adapted to the new rule and to have them race together in international competition. It was in this context in 1925 that Virginie Hériot, the renowned French sailor, challenged Italy to defend the Italian Cup, the most prestigious Italian sailing trophy. She came with *Aile IV*, her Eight Metre. The Italian defender was *Chete*, helmed by Francesco Giovanelli.

At one point in a difficult and tense race, *Aile IV* was ahead as it rounded the buoy for a new windward leg. *Cheta* followed close behind. The Italians knew that they would be forced to suffer the spoiled wind left by *Aile IV*. To avoid this, they tacked away without changing

their downwind jib. This meant sailing further off the wind and thus lengthening the distance to cover. This tactic enabled *Cheta* to close the distance and to arrive at the windward buoy almost at the same time as *Aile IV*.

This manoeuvre was duly noted by Raimondo Panario.

During the following fall, with his assistant Francesco Tagliafico, he experimented continually. He wanted to fabricate a new windward sail based on the balloon jib which he would introduce during the winter regatta in February 1926. At once a technician, a helmsman, and a sailmaker, he knew how to evaluate and distinguish the positive and negative effects of overlapping the balloon with the mainsail. Starting with the panels of a second hand British sail, he fabricated a large jib with little curvature, designed to function to windward. Twenty days before the first of the Genoa winter races, he tested it on the new Baglietto Six *Cora IV*, ordered by Giuseppe Roggero. Raimondo Panario was convinced that this sail would produce an aerodynamic acceleration to windward. Lacking a scientific background, he was driven only by intuition based on hands-on sailing experience. It was only once the design had become a proven success that scientists could explain, after the fact, that the interaction of the new jib and a mainsail, which had been flattened, produced the well known aerodynamic phenomenon known as the “Venturi effect”.

For Panario, 1926 became an unforgettable year. French, British, Dutch, Norwegian and Danish boats arrived for the Genoa international races. All were beaten by *Cora IV* which deployed her new sail plan (a new jib carefully matched to a relatively flat-cut mainsail).

Wishing to try to beat *Cora IV* on their home waters, the Danes invited the Italians to the international regatta of Copenhagen. In Copenhagen on July 22 - 25, *Cora IV*, shone again against a fleet of twenty Sixes, taking two first and a second. The King of Denmark, Christian X, even invited the Italian crew to lunch on the royal yacht. Enchanted by Panario's abilities, the king referred to him as the “first skipper of Europe”.



Cora IV with genoa

A year later, the best Finnish, Danish, British and Italian yachtsmen converged on Oyster Bay (New York) with new Sixes to compete for the Scandinavian Gold Cup. The Swedish skipper Sven Salén on *May Be* carried a big windward jib based on the sail which *Cora IV* had introduced at Genoa. He too won hands down. He attributed a major part of his success to his new foresail, immediately called a “Swedish jib”. He explained that it allowed him to use new tactics during the regatta.

Not surprisingly, the modest sailmaker of Voltri became the object of intense jealousy. People tried to discredit his invention. The facts were twisted and malicious interpretations emerged. Critics in Italy ridiculed his invention, giving it the disparaging name of the “Sloppy Bandage”, accusing it of breaking spreaders and masts.

To escape the pain, “the first skipper of Europe” went into permanent and self-imposed exile, leaving Genoa and retiring to the village of Belgirate on Lake Maggiore, 150 km to the north.

Fortunately, the great yachting architect Clinton Crane, one of the pillars of American yachting, clearly attributed the Genoa jib to Panario in the now famous paragraph of his *Yachting Memories*. Furthermore, the Americans, clearly understanding the importance of Panario's invention, improved on it and extended its performance.

Raimondo Panario knew nothing of this belated recognition. His positive memories of the 1925-26 period, before the controversies erupted, included a possible scenario entirely different from his lakeside retreat. The great Italian yacht builder, Vittorino Baglietto, had

proposed taking him on as a partner at Varazze (40 km to the west of Genoa) as head of a new sailmaking section of his yard, saying, "Raimondo, when it comes to sails, you know as much as Ratsey and maybe more."

At Belgirate, Raimondo Panario sewed his last sail at age ninety. A few days before his death, maintaining his standards of quality and excellence, he tested it personally at the helm of a Star.

*Jacques Dumon*, who sails his Six *Fissa* at Cassis near Marseilles, is a regular contributor to the newsletter.

### **Rescue Opportunity for 2008 and candidates for the years ahead**

Our Rescue Opportunity for the year is a handsome Six currently in Italy. For a while, it was thought she might be one of the *May Be*'s, but none of the missing *May Be*'s seems to match its lines. Somewhat jokingly, she has thus acquired the unofficial name of *May Be Not*.

An Italian naval architect who has examined her reports:

"The diagonal lamination is opening and clearly visible under the topside varnish. She needs a new deck, the whole main section is lifting up because of the shrouds. There is a lot of rot in the structural parts. The deck and hull are leaking badly.

She has an engine. I am also convinced that the original stern has been cut off.

*May Be Not* certainly has beautiful lines.

The only evidence as to her origins remains the number "S 8 2 3 5" carved in a main section frame close to the mast step. The "S" could also be a "9".

She is somewhere in the Lake Maggiore region. (The naval architect knows where she is.) We have no information as to an asking price.



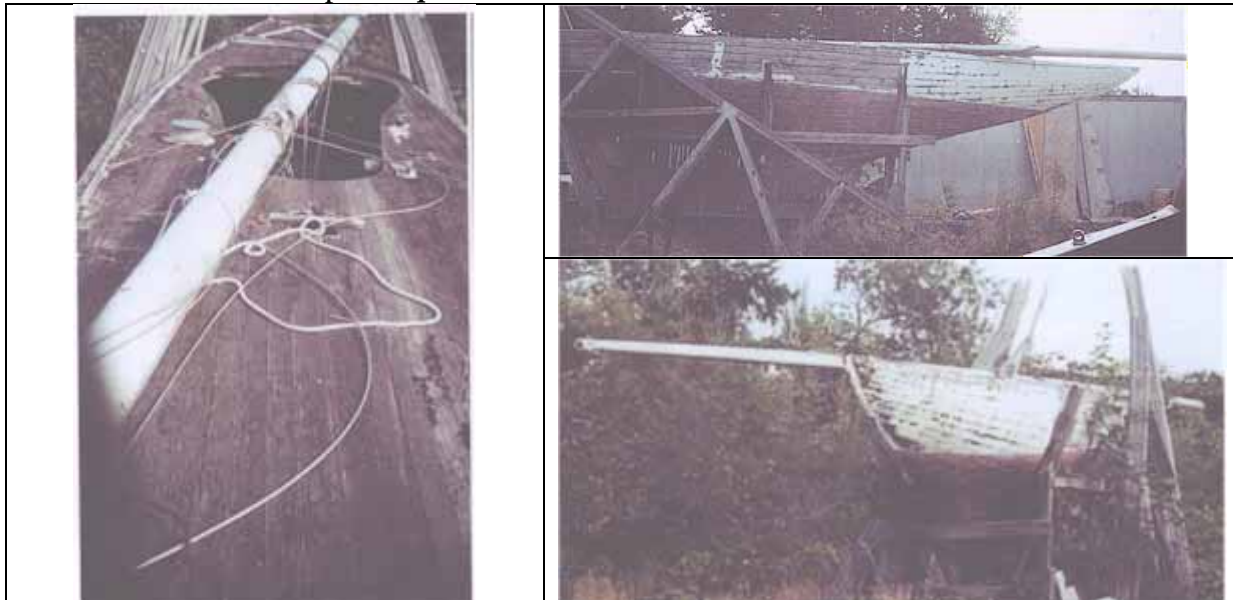
As for candidates for the years ahead, they are all high quality boats with good racing potential. Two are in Europe and six in the U.S. From East to West, they are:

- France: FRA 60 *Izénah II*, perhaps the most successful of Louis Bréguet's designs, built by Macario in 1934. She's been outdoors uncovered for more than 20 years. (The editors don't have a photo of her.)
- Ireland: K 25 *Susette*, a 1928 Fife design built by McGruer. She is located at George Hegarty's boatyard at Monkstown, Co. Cork Ireland. Her owner is prepared to sell.



- North America
  - Massachusetts: US 33 *Clytie II*, a Clinton Crane design, built by Nevins in 1927.
  - Puget Sound N 61 *Lully II*, a 1936 Bjarn Aas design and build.
  - Puget Sound US 66 *Indian Scout*, a Herman Whiton design, built by A.E. Luders in 1935 currently at Port Townsend, Washington.
  - Puget Sound KC 6 *Buzzy II*, an Arvid Laurin design, built by Neglinge in Finland in 1948.

- Puget Sound K 49 *Noa*, a 1947 design and build by James McGruer – and a sister ship of *Caprice*.



- Puget Sound US 64 *Tao*, a combined Olin Stephens / Herman Whiton design, built by A.E. Luders in 1934 currently at Port Townsend, Washington. Her frames were badly damaged during transport by truck across the U.S.



### **K 45 Kyla needs sails**

In last year's newsletter, we reported on the unique circumstances of the restoration of *Kyla* by the Conservatoire Maritime du Havre (France).

The Conservatoire has received funding from local welfare agencies to re-habilitate and train a group of long-term unemployed individuals. Their training consists of the restoration of *Kyla*.

The funding received by the Conservatoire covers personnel costs, but not materials.

After the WC 2007, an appeal was circulated for a mast and winches. Two owners of Sixes, who prefer to stay anonymous, rapidly responded to this appeal and contributed a wooden mast and a complete set of bronze winches.

The Conservatoire is now in need of a set of used sails.

Because of the strong support of the Six Metre community for *Kyla*, the Conservatoire would like its next project to be a Six too. Preliminary discussions are already underway to this effect. If it materialises, a second set of sails would also be required.

Owners intending to participate in the EC 2008 at La Trinité are encouraged to bring their used sails for donation to the *Kyla* project.

## **Flash – new Cannes yacht club organising metric yacht races in September**

Despite the withdrawal of the Yacht Club of Cannes (YCC) as host for the 2008 European Championships, the French Six Metre association (the UF6mJI) has maintained close contact with its friends in Cannes, the supporters of metric class racing at Cannes. These latter have since revived the **Union du Yachting Cannois** ([www.uyc.fr](http://www.uyc.fr)) which had merged with another club in 1930 to form the YCC.

The new UYC has just been authorised by the French affiliate of ISAF to organise a regatta for all the metric classes from **Saturday 21<sup>st</sup> to Tuesday 24<sup>th</sup> September, 2008**, during the week preceding when the Régates Royales are traditionally held.

This information was received by the UF6mJI. It will keep Six Metre owners informed of developments regarding this regatta.

## **North American Six Metre Web site**

The editors wish to congratulate Matt Cockburn for his exemplary site which he which he continually improves. Under the “Fleets” heading, the North American boats are divided into six fleets. For each fleet, the constituent boats are listed. Clicking on a boat name opens its dedicated Web page. The pictures are high quality and the length of the texts is just right. It’s a delight to wander through the site. (see [www.6mrnorthamerica.com](http://www.6mrnorthamerica.com))

## News from the Fleets

### 1. S 7 Agnes IV (K.Ljungberg, 1911), by Peter Vikeby (Norway)

*Agnes IV* is doing well. :)

She is under a tent on land for the winter, and some work is being done - lot of finishing, both on the outside, and inside. She was doing quite well in the water last summer.

My family and I sailed her the whole summer, and she was "waterproofed" after 3 weeks in sea. That was very good, since it was 10-15 years since she was in water the last time.

In my renovation last winter, I built a lot of new frames, new "røstjern" (for the vant) and changed all the old steel frames to AIS 316. I had to put in 2 x 100 cm. in the hull, but that was all. The soon-to-be 100 year old mahogany is still in ship shape!

I made a completely new deck and roof over cabin...in plywood with epoxy top. Since this was my first time doing that kind of work I was surprised when it actually was watertight - also from the top.

I had many beautiful days on the sea here in Norway with this old lady - still called Agnes IV - and I'm looking forward to many more

Length: 9,78 m

Widdth: 1,70

Constructor: K. Ljungren - Sweden

Built: 1911 - Hestholmsverftet, Stockholm - Sweden

I have attached some pictures - one from 1911, one from when I first saw her in bad condition after 15 years on land and one from when I sailed her last summer. She will look a lot better this summer, but she got water under her keel last summer - for me an important thing!



ÅGNES IV.  
K. S. S. S:s utlottningsbåt 1911.





## 2. **GER 59 Aida** (B. Aas, 1936), by **Katrin & Bjorn Storsberg** (Germany)

2007 was a very successful year for *Aida*.

After finishing the rebuilt in 2006, there was not much work left on the boat. So, we concentrated on improving our speed and participated in local races for practice.

The highlight of the season was to come in 9th in the World-Championships at Cowes. Even if we thought that we could have done better, to be 9th in those difficult conditions and against lots of other very good crews was more than satisfying.

At the end of the season, *Aida* won the Jubilee-Race of the 100<sup>th</sup> anniversary of our local sailing-club (Arniser Segelclub). For us it was a great success, because this "old lady" did win against one hundred modern yachts.

For 2008 it seems that we won't participate at the European Championships (date is not suitable), but we plan to come to southern France in Sep/Oct.

Maybe we can arrange an Open German-Championship in August in Kiel.



## 3. **GER 44 Ayesha I** (J. Anker, 1912), by **Peter Bartl** (Germany)

My skiing career lasted 65 years without any critical accident, but finally it happened. I am recovering now and I will soon try again in Austria. Since the accident, *Ayesha* is waiting in the boathouse to be revived.

When she was put into storage, I had some correspondence about her with the Royal Secretary of King Olaf in 1986.

There is some indication of her having raced in Sweden, but no proof. We will probably never know for sure who her first owner was. When Mr. Magirus brought her to the Bodensee from Norway in 1918, he put the news out that he had bought her from the King.

He reported that she was built in Norway 1911-1912 by Johan Anker for the Royal Family to participate in the 1912 Olympics. Her first name was *Sonja II*, but it is not proven that she finally participated.

When she was owned by the Magirus Family, she had the name of *Hadumoth*. Later in 1936 she was named *Ayesha* and she has ever since stayed on our Lake.

I am her Master since 1984 and our boatbuilder has restored her completely to become a beauty again. My wife and I can sail her safely on races and leisure trips.

We will be back on the lake with her on May 15th.

I am also looking for a new owner for her since I am now retired. [*Ayesha* is listed for sale at the end of this newsletter with a complete set of photos.]

## 4. **Z 16 Azais** (F. Camatte, 1933), by **Paul Bos** (The Netherlands)

I re-started my restoration work on *Azais* at Christmas of last year after a delay of more than a year. Because of other obligations, I couldn't find the time and she stood in the boathouse. The positive consequence of this is that she is now very dry - a good start for re-building.

All the paint has been removed. About 30 frames will be installed. By the end of January about 3 m<sup>2</sup> of planks will be replaced. Then the hull should be ready for another 30 years!

The condition of the deck is still reasonable - good enough just to re-paint it with its 2006 colours.



My aim is to have Azaïs back in the water by May 1<sup>st</sup>. I will then have three weeks to install mast and sails and to practice before we hold the first regatta for Six Metre yachts in Holland since World War II.

Next winter, I plan to install a teak or yellow pine deck.

**5. FIN 65 Bambi** (K. Reimers, 1946), *by Klaus Ilmonen (Finland)*

*Bambi* mainly took a rest from competitions in 2007. She is now preparing for the 2008 season with a new crew and helmsman. She will be sailing with NJK in Helsinki as her home port.

**6. SUI 70 Belanea** (D. Boyd, 1947), *by Andrea Roost (Switzerland)*

The boat is still sailing on the waters of the lake of Thoune in the Bernese Oberland. She is still for sale, price to be discussed.



**7. US 88 Blodoks** (E. Kristoffersen, 1946), *by Dana Olsen (U.S.A.)*

About April, I hope I will begin the full restoration of Blodoks. The new shop I am building was slowed by bad weather this winter. The roof is going on now

**8. F 23 Cailin** (?), *by Sean Cullinan (U.K.)*

*Cailin* and “*May Be Not*” (Rescue Opportunity of the Year) are two *bona fide* Sixes whose origin is a complete mystery. She is currently at Ipswich and competed in the 2007 Worlds.



## 9. GBR 48 Caprice (J. McGruer, 1946), by Paul Smith (U.K.)

*Caprice* finished third at the Worlds against some very good quality competition and in conditions that on one afternoon in particular were very testing of both boat and crew (30 knot gusts on the last down wind leg).

At the end of the year, the yard where she lives (Universal Marine) undertook a significant program of cosmetic restoration which meant a very new and shiny paint and varnishing job. Once this was done, she was exhibited at the London (Earls Court) Boat Show.



## 10. FIN 2 Caresse (Z. Westin, 1922), by Thorsten Koch (Germany)

Some of the information below was published when *Caresse* was displayed at the Helsinki boat show in 2001.

*Caresse* was commissioned by the yacht club "Airisto Segelsällskap" (ASS) in 1922. She was to be raffled and Jarl Cederqvist won the draw. He sold the boat to the former owners' grandfather, Sigurd Blomqvist, who raced her successfully the first season in 1923. *Caresse* was the first yacht to be built in Finland according to the Second Rule of 1919.

She was sold to Sweden after WWII, after having had several owners. Since 1955, she was the pride and joy of the Swedish general, Gunnar af Klintberg, whose son Tosse inherited her in the late 80's. Prior to restoring her, she turned over on land and was badly damaged. Tosse sold her to a boat builder in Stockholm, Svante Håglund, who kept her on land with the intention to restore her one day.



Jan Blomqvist managed to track her down in 1998 and found her in a dreadful condition, beyond restoration. Her stern had been amputated of 1 metre. She was then transported to Lovisa, Finland, to be rebuilt by M-yachts.

Despite thorough investigations, the original drawings could never be found. A naval architect measured the hull by laser and thus produced new drawings (see article "Plans for every Six above). The old hull was then cut to bits in order to get access to the iron frames of which a number could be restored and used for the new hull. The hull was built using the same materials and techniques as the original one of 1922 - lead keel, mahogany planking on

steel and oak frames, Oregon pine decks and silver spruce mast. Caresse is also equipped with bronze winches from Meissner.

Caresse was launched again in 2001 and sold to Germany a little later to be mainly sailed at lake Constance more than 85 years after the first owner took possession.

**11. CAN 8 Carin II** (G. Kyntzell, 1937), *by Chris Wittstock (Canada)*

*Carin II's* first name was *Alic* and then *Carin*, possibly after Count Julenberg's wife.

She went to Canada early 50's. Her name changed to *Carin II*, supposedly because there was already a boat called **Carin** on the registry ( not a 6).

She is as usual at Lallows, Cowes.



**12. US 65 Challenge** (A.E. Luders, 1934), *by Michael Durland (U.S.A.)*

*Challenge* is owned by Kat Fennell and me.

We were a bit behind the curve against the other 6's at our regatta on Orcas Island last September, but I can attribute that 10% to a used genoa and 90% operator error.

Challenge is back in the shed for the winter and will get the final hull fairing and a shiny coat of paint this year. Last year we were so excited to go sailing (after a restoration), we did not fair the hull.

We may start the season at the Victoria B.C. Swiftsure end of May and the fleet will have a separate course at Port Townsend the first weekend in June at the Classic Yacht Regatta.

**13. US 53 Cherokee** (O. Stephens, 1930/2008), *by Terry Nathan (U.S.A)*

The Museum of Yachting of Newport, Rhode Island, the organising authority for the 2009 World Cup, has commissioned the reproduction of *Cherokee* which is offered for sale at \$160.000 (see [uk.yachtworld.com/sparkmanandstephens/index.html](http://uk.yachtworld.com/sparkmanandstephens/index.html)).

It may be the first time ever that a Six has been commissioned for sale to a third party. It is a wonderful initiative and an ideal opportunity for someone looking for a Six to race in the 2009 Worlds.



Photo courtesy of Sparkman & Stephens

**14. I 64 Ciocca II** (O. Stephens, 1948), *by Brian Pope (U.K.)*

*Ciocca II* went to the London Boat Show , no keel or lower planks , a bit like *Joanna*. (see [www.metreyacht.com/sales/cioccai.htm](http://www.metreyacht.com/sales/cioccai.htm))



## 15. **D 22 Clarity** (W&R Fife, 1924), by *Bill Doyle* (U.S.A.)

Jed Pearsall should be listed as the owner of *Clarity*. I am his partner / co-owner. He also owns the NY-30, *Amorita*, that was run-down by *Sumurun* last summer.

Jed purchased her in Maine roughly 30 years ago and ,along with his father, restored her to beautiful condition, preserving nearly all of her original fabric. (His father was a famed furniture designer, who restored the Fife *Halloween* back to life in the early 1990's.) *Clarity* was Jed's first classic sailboat, which led to many more restoration and preservation projects.

Under her original name was *Bonzo*, she won a silver medal in the 1924 Olympics. She was re-named *Clarity* in the early 80's because of the pejorative association of the name *Bonzo* with an American clown character "Bozo" that denotes someone who is clueless, ignorant, or reckless.



After the Olympics, she competed in the States briefly among the 6's and then was sold off as a pleasure boat and given a modest cabin in the early 1930's which she has kept ever since.

She was sailed many years in Maine and eventually sold. While she has had constant work completed on her, she has never gone through a top to bottom, full frame by frame, restoration. Instead she has been well maintained throughout her life and has received great deal of care to keep her launched and race ready, season after season. She has nearly all her original planking, frames, etc.

This coming season, as with the past decade or so, she will be moored at her home port of Newport, RI, and will be competing in both the local classic events, as well as several Performance Handicap Racing Fleet (PHRF) races, where she out-points nearly every modern competitor she comes up against.

She is currently maintained by MPG in Mystic Connecticut, where she is stored indoors in the off-season. As Mystic is about 30 n.m. distance from Newport, she makes her twice annual off-shore cruise for delivery and de-commissioning.

While she may not be at the top of the Classic 6 fleet for next year's Worlds, she seems to be an excellent heavy weather boat, deep and narrow as Fife liked to design them, and she will be competing as a happy participant none-the-less.

In 2008, *Clarity* will be joined by fellow classic 6's for the first time in decades. We'll see how she goes.

**17. FRA 109 Clyde** (W. Fife, 1955)

Clyde is well and is patiently awaiting the 2008 racing season in its storage shed at Bénodet (Brittany).

The photo was taken in June 2007 by Erwan Quéméré during the Belle Plaisance classic boat regatta with winds of about 20 knots.



**18. E 22 Colibri** (P. Arbaut & A. Godinet, 1927), *by Paco Sánchez (Spain)*

*Colibri* is sailing perfectly and providing us with a great deal of satisfaction. She is berthed at the Real Club Nautico de Valencia.

For the weather conditions of Valencia, *Colibri* is an ideal day-sailing boat and we go out often.

She just celebrated her 81<sup>st</sup> birthday, having been in our care for the last 20 years.

**19. B 66 Clymène II** (B. Aas, 1959), *by Bernard de Wasseige (Belgium)*

*Clymène II* was dismasted during the 2007 Voiles de Saint-Tropez. A new mast is under construction, so she'll be ready for the 2008 season.

**17. POL 4 Dana** (M. Giles, 1927), *by Darek Dziwura (Poland)*





## 20. **US 80 Djinn** (O. Stephens, 1938), by **Henrik Andersin** (Finland)

*Djinn* had a full season 2007 and participated in all 6 Finnish events with various successes. In the National rankings, she came in 4<sup>th</sup> of a fleet of 20 Sixes starting throughout the season.

The canvas deck is now three seasons old and it does not show any aging or leaking. We have not repainted it since it was rebuilt to keep it from becoming slippery but I would expect it to need some paint within two years. The underwater hull needed some fairing where the lead keel touches wood.

The boat feels good now.

The hull needs touch-up paint as *Djinn* has been campaigning and travelling quite a lot. Some of the brass hardware has failed and needs replacement.

The Sitka Spruce mast is not completely straight anymore. It shows some “pre-bend” if you look closely. I have therefore commissioned a reserve mast to be built. This new mast will only be used if the old



one fails. John Lammerts van Bueren furnished perfect Sitka Spruce for the new mast and I believe the raw material of the new mast is of superior quality.

*Djinn* has a light Cuben Genoa for 1-5m/s (2-10 knots) use, but Southern Finland only had hard winds in 2007. This is the first time that a non Vintage Classic tradition sail has been used and time will tell if I will use it or not. Cuben Fibre might be too stiff for a wooden mast.

**21. BEL 17 Edelweiss II** (L. Hope, 1914), by *Werner Huybrechts (Belgium.)*

I haven't had much time recently for the restoration of *Edelweiss II* - and there is still a long way to go.

I attach a picture from 1919, just after she won the "Six Cup" on the former "Zuiderzee". That is how she should look like again ....someday.

The Zuiderzee was a sea in Holland which, after being closed off from the North Sea, is now called " Ijselmeer".

Another photo from 1914 proves that *Edelweiss II* was designed with a Bermuda sail (marconi rig). Linton Hope was one of the first to promote and defend the Bermuda rig. As early as 1913, some Sixes were converted from gaff-rigged to Bermuda. I am still trying to get the original sail plan from Ratsey & Laphorne. They say they probably still have it "somewhere"!



*Edelweiss II* always kept her name, except that there is an unconfirmed story that she was called *Diana* during WWII.

**22. KC 9 Elisabeth X** (B. Aas, 1947), by *Hans Oen (U.S.A.)*

*Elisabeth X* is basically ready for serious competition again. Her last major appearance was at the EC 2002 in Copenhagen.

The East Coast of the US is now really humming with activity surrounding Classics. Replicas of *US 56 Jill* and *US 53 Cherokee* ( S&S) will both be finished by spring, so will Jessie Smith's *US 51 Tote"* (Luders) 1930 ( modified to 3rd Rule).



We are planning to kick off the season with a 6mR regatta in Western Long Island Sound in late May, sponsored by Seawanhaka Yacht Club. In addition to the boats mentioned above, *Alana*, *Flapper*, *Elisabeth X* will participate with the possibility of a couple more.

The photo is of *Elisabeth X* at Oslo in 1950.

**23. NED 25 Fiona (W. Fife, 1935), by Jan Willem Ypma (The Netherlands)**

*Fiona* was refitted in 2007 by Frederik Heerlien, at his yard "Heerlien Klassieke Schepen", ([www.heerlienkls.nl](http://www.heerlienkls.nl)). He also refitted *Piccolo*, another W.Fife 6mR, located in Holland.



*Fiona* was the last 6mR built by W. Fife & Sons.

**24. FRA 120 Fissa (P. Baglietto, 1926), by Jacques Dumon (France)**

Following up on your call for help to all owners, in last year's Newsletter, for information on the five Sixes stranded in Oran, Algeria in 1962, a business trip took me to Algeria in January. During the trip, I met a French citizen who lives and works there and explained the search to him. He seemed interested. At this point, we can only hope that he uncovers something.



The photo shows *Fissa* sailing on the Mediterranean close to the cliffs that line the shores around Cassis, her home port.

**25. US 90 Fokus III (B. Aas, 1948), by *Jim Metteer* (U.S.A.)**

*Fokus*' three year restoration includes: the removal of the ballast from the keel timber, all new floors throughout the mid-section and 90% new steam-bent frames, a new deck of yellow cedar and teak over marine plywood.

*Fokus* has been restored with new custom built 1950's style aluminium winches. She hopes to have a new modern-style wooden rig.

This sister-boat of *Elizabeth X* will sail for the first time in 23 years, with the goal of racing in the '09 World's in Newport, RI.

**She was saved by Harry Hoffman, Dan Taylor and Kimo Mackey of Bainbridge Island, Wa. and the efforts of local community craftsman. Harry found her and began restoration. Dan Taylor preserved her. Kimo Mackey (owner of Saga) brought Dan and me together. Dan gifted the boat to me. And with this magnificent act of generosity, the restoration began.**



**26. US 80 Goose (O. Stephens, 1938), by *Peter & Martha Hoffmann* (U.S.A.)**



Goose at Cowes in 1949 when she participated in the U.S. win of the British American Cup

## 27. **D 48 Hakahala** (K. Reimers, 1935), by Jan Mateboer Celsus (The Netherlands)

*Hakahala's* re-build continues to progress. We are now working on the deck.

We first dried the plywood with a hot air blower, then we finished it with 2 layers epoxy. We are now busy gluing the small teak strips to the deck. It will look lovely when *Hakahala* is at the dock!

**She has to be ready for the Dutch championship in May.**



## 28. **GBR 3 Houri** (A. E. Payne, 1911), by David Seer (U.K.)

*Houri* is currently at the Aldeburgh Boat Yard in Suffolk, in the capable hands of Peter Wilson and his highly skilled team. She has been there for something approaching two years.

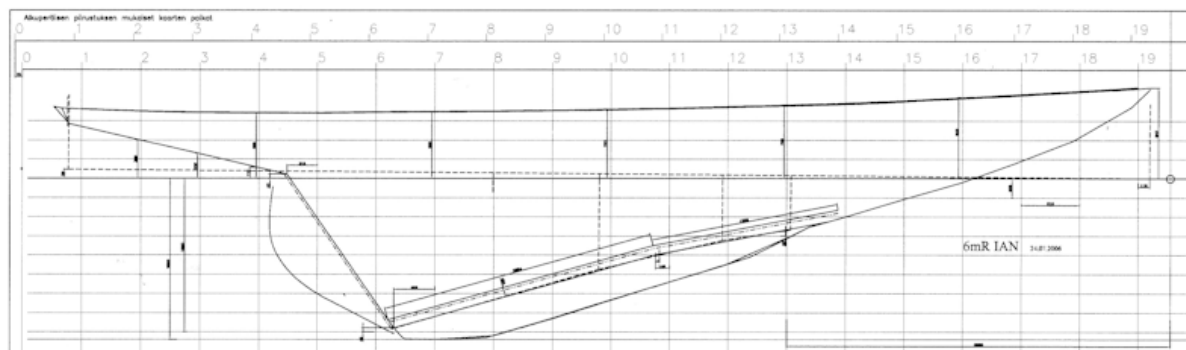
In that time, the remains of her deck have been removed, together with the rudder, ballast, deadwoods, and all of the internal steelwork. The aft end of the keelson has been removed, for about a third of its length, and a brand new section has been grafted on to the original. The lower end of the stem, and its joint with the keelson, has been substantially reinforced with new timber. Both garboards have been replaced, together with all of her frames, and she has been completely re-fastened.

When I bought her, she had a *retrousee* transom, and had apparently been shortened by about a foot. So in the complete absence of any original drawings, we used the earliest available photographs from Beken of Cowes to restore her counter and transom to as close to original as possible. Her overall length is now somewhere in the region of 30' 6", which from what I know of her, is about right. In addition to all new laminated floors and two ring frames, she has also been given new beam shelves. Then, before she was put back outside for the season, the remainder of her original caulking was removed, and replaced.

She was brought back inside during October, shortly after which I, together with a skilled gentlemen, a laser and a lap-top, paid her a visit and set about the task of scanning her and creating a 3D model of her - all in the name of engineering, and hopefully improving her performance ! I'm hoping that there will be an article about the operation in *Classic Boat* in the near future.

### Rebirth of a second rule classic 6 Metre yacht

*Ian* was launched in the spring of 1929 in Gothenburg, Sweden. She was the last Six Metre yacht that the Finnish-born naval architect-designer-wizard Gustaf Estlander built for himself. Mr. Estlander was at the height of his career and *Ian* is considered to be one of his masterpieces. She was for several years one of the top yachts in Scandinavia, winning, among other trophies, the Gold Cup in 1930, thus joining *May Be* and *Ingegerd* as the third Estlander design to win this coveted trophy.



As many racing Sixes, *Ian* has had a tough life over the years. In 1955, Sten Fernstedt had a copy of *Ian* made to the original drawings, and fitted the new hull with *Ian's* original keel. This copy, *Mirage*, is now at Versoix on Lake Lemman. After sailing for some time with a concrete(!) keel, the old *Ian* got a new lead keel in 1967.



In the autumn on 2005 it was decided to refurbish *Ian* professionally: the teak deck from the 70's was in bad shape, most of the ribs were broken, she leaked badly etc. From the start the "guiding light" was to restore the yacht properly to as original a state as possible. She regained her original hull shape and the deck layout. When the keel was dropped it was quite a shock to see that on top of the lead was a thick layer of different fillers. Comparing it to the original drawings, the shape was completely wrong. When Finnish boat designer Kamu Stråhlman transferred the original line drawings to his computer and compared these with the measurements taken from the hull, we were able to reframe the yacht to her original shape. Mr Stråhlman also designed a new keel to the right Second Rule original shape so that *Ian* now looks exactly as Gustaf Estlander first saw her so many years ago. The deck layout follows the original design closely, but with some alterations to allow a 5-man crew to race her efficiently. Equipped with a new set of sails, she will hopefully show her younger 3<sup>rd</sup> rule sisters her pretty little transom, racing out of her home port in Helsinki.

Sam Rowse and I have commissioned a complete re-build of Jill, only keeping her lead keel. Olin (Stephens) is in the loop and making sure we get everything right.

Work started on 20th August 2007 and Rockport Marine is running ahead of schedule. I am trying to think of a good Spring launch day - maybe a day important in Jill's history



(Editor's note: the Jill re-build project has set a new standard for Six Metre "visible construction". Never has a Six Metre restoration/re-build project provided such a complete set of step-by-step publicly available photographs. In the absence of an ISMA-mandated construction survey service such as *Lloyds* and *Veritas* performed previously, Jill's "visible construction" approach is probably the best solution for providing a guarantee to the Six Metre community that her re-build has faithfully used traditional materials and construction techniques.

Martha Coolidge's Web site is [web.mac.com/marthafaycoolidge/iWeb/Site/Jill.html](http://web.mac.com/marthafaycoolidge/iWeb/Site/Jill.html) and Rockport Marine's [www.rockportmarine.com](http://www.rockportmarine.com). The photographs have been circulated on a monthly basis by Matt Cockburn ([matt6mr56@comcast.net](mailto:matt6mr56@comcast.net)).

### **31. L 42 Leila (w. Fife, 1912), by Mike Dalglish (U.K.)**

*Leila* is just starting her refit now and it is going to be a lot more extensive than what we first thought. We found quite a lot of rot in her, stem and sternpost mainly.

It will be a few years before she graces these fine Scottish waters again, hopefully in time for her centennial, 2012.

**32. GER 68 Lillevi (Z. Westin, 1938), by Oliver Berking (Germany)**

I am restoring a 1939 Twelve Metre, *Sphinx*, which needs to be ready for next spring. We will need the spring to practice 12 Metre racing (which I've never done before) to be prepared for the World Championship from July 5 – 11, for which I am responsible (see [www.robbeberking.com](http://www.robbeberking.com)).

Despite this new adventure, I will not sell *Lillevi*, but I hope that my fellow Six Metre sailors will understand why next summer will be a 12 Metre summer for my crew and me.

I wish you a big fleet and a good championship at La Trinité.

Here follows an excerpt from the *Sphinx* Web site ([www.sphinx-12mr.de](http://www.sphinx-12mr.de)):“After the war, the Freudenberg brothers sailed her with the name *Lobito*, winning in 1948 the traditional race for the “Blaue Band der Niederelbe”. Ten years later, in 1958, *Sphinx* was sold to the Naval College of Mürwik which used it until 2004 as a training ship, under the name *Ostwind*. In the 1960's and 1970's, the Navy won the “Blaue Band der Flensburger Förde” for the fastest local yacht a total of 9 times.

On September 30, 2005, both 12mR yachts of the Naval College of Mürwik, *Ostwind* and *Westwind*, were sold by auction.

The citizens of Flensburg had tried everything to avoid this loss to their local yachting heritage. When the auction date was fixed, three Flensburg sailors, Jochen Frank, Gorm Gondesén and **Oliver Berking**, decided to make an offer to save at least one of the yachts. They were successful! In January 2006, under direction of Kai Wohlenberg, the restoration of *Ostwind* started. She will henceforth bear her original name *Sphinx*.”





**33. SWE 1236 Lisbeth V** (J. Anker, 1932), *by Johan Larson (Sweden)*

*Lisbeth V* is now in Hamble in the UK. I will sail her in the British Open in Plymouth and then again at La Trinité. After the EC, she will probably go back to Sweden again.

**34. US 83 Llanoria** (O. Stephens, 1948), *by Peter & Martha Hoffmann (U.S.A.)*



**35. US 55 Lucie II** (C. Crane, 1931), *by Greg Carroll (somewhere)*

I am currently on a circumnavigation of the globe and don't expect to commission *Lucie II* until the spring of 2010.

36. US 72 Lulu (O. Stephens, 1937), by *Craig & Nicole Downey (U.S.A.)*

We launched *Lulu* on August 10, 2007 after a restoration that included frames, floors, planking, mast step and decks. The boat had been altered from her original plan for cockpit positioning and mast location. I am currently building her sails to fit her original sail plan. We hope to have her sailing this spring.



37 US 21 Madcap (F. Hoyt, 1924), by Karl Cressotti, 2<sup>nd</sup> year student, International Yacht Restoration School, Newport, RI (USA)

MADCAP Restoration

The International 6 Metre, Madcap (US 21) is currently undergoing restoration at the International Yacht Restoration School (IYRS) in Newport, Rhode Island, USA. Madcap is an early two-cockpit design from the board of Frederick Hoyt. The yacht was constructed in 1924 at the Nevins Yard in City Island, NY. At 22 feet on the water, and with almost 7 feet of beam, she is one of the smallest 6 Metres of her period. She may also be one of the best preserved.

Early in the vessel's career, *Madcap*'s ballast position was modified. Close inspection at the beginning of the restoration showed evidence of two replaced planks on her port side, but her original fabric was largely intact and in excellent shape. The garboards and first boards were sacrificed to allow thorough inspection of her floors. Of her fifty-two floors, IYRS has replaced one, sistered one with bronze, and patched three more. Twelve of her 104 frames had to be replaced. The deck needed a complete rebuild.

*Madcap* had been modified with a cabin and self-bailing cockpit sometime before 1949, according to Marilyn Williams, the vessel's previous owner. A trunk cabin had been created

over the original winch pit by extending the opening forward to the after beam in the way of the mast partner (an increase in length of approximately two feet). The hole was also widened from the original two feet to five feet. The after cockpit had been altered to follow the lines of the cabin trunk, with bench seating on either side, and the rudder shaft extended to bring the tiller up out of the boat.

In 1960 the deck was stripped of its canvas and fibreglassed, according to Williams. The coamings and cabin trunk were clear-coated as well.

When IYRS began restoration of *Madcap* in September 2008, the intention was to return the deck to its original configuration. The built-up bulwarks were cut down and the fiberglass chiseled from the deck, revealing her original mahogany covering boards and sprung cedar strip decking, ends herringboned together. These were removed with great care, so that the deck structure might be saved. However, the iron fastenings of the decking had done-in the spruce and oak beams, which had to be replaced.

The new beams were laid, preserving the original camber of the deck (which grows a quarter of an inch in the way of the mast partner and continues forward to the bow). The new oak carlins were let into the oak beams defining them with bevel-lapped, half dovetails, as were the half-beams into the carlins. New quarter knees have been installed and the original, galvanized hanging knees have been sealed with epoxy to prevent any future electrolytic action.

A strongback has been run down the vessel's centerline, cross-lapped into the deck beams. This was deemed a much better way to provide support to the decking joinery than the original, which consisted only in a series of toe nailed backer blocks. Laminated coaming knees have just been faired into the helmsman's pit in preparation for decking.

The deck will consist of 5/8ths inch sapele mahogany covering boards let into the deck beams so as to be flush with the 1/2 inch cedar strip planking. This is in accordance with the vessel's original construction and Nevin's own rules for yacht construction, which allows for a 1/8<sup>th</sup> inch reduction in decking thickness where the deck is to be canvas covered. The coamings will also be sapele.

*Madcap* is being restored for a syndicate out of Newport, headed by Tom Fair, a local businessman, sailor and racecar driver. The restoration is being done by 7 second year students at IYRS, under the direction of Warren Barker. Prior to construction of her new deck, a survey by Pedrick Yacht designs deemed *Madcap*'s hull to be true to her original specifications.

Although refastened, from keel-bolts to planking screws, *Madcap* retains a remarkable amount of original fabric. She will be an exquisite restoration of a classic yacht, true to the early 6 Metre rule.

### 38. GBR 1 Maida (J. G. Stephen, 1932), by *Richard Rankin (U.K.)*

*Maida* was designed by J G Stephen and built by Alex Stephen & Sons, Glasgow in 1932 for Lord Caenarvon.

J G Stephen was an innovative designer in his day and one of the pioneers of tank testing designs. As a result of his tests, *Maida* has fairly extreme lines, the stem forming an almost straight line from the heel of the keel to the forward water line, with a slight knuckle up to the stemhead. The hull is unusually full before the mast and the counter is long and very fine, ending in a small raked arch-board. Originally *Maida* also had an unusual mast design, with a narrow, aerodynamic forward edge, almost the reverse of the conventional shape.

The original construction was obeche hull planking (described on the original drawings as African white mahogany) on a yellow pine back-bone.

*Maida* is famous for having lost the Seawanhaka Cup to Jill in 1932.

In later life, a dog house and an engine were added to convert her for cruising. She also spent an extended time onshore in one owner's garden.

The rebuild was begun by Scottish boat builder David Spy who replaced all her frames and some planking.

The restoration was completed by Peter Wilson in Aldeburgh using the original plans. Some design changes were made and some modern techniques and materials

used, but the spirit of the original design has been retained.

Collars custom made the wooden spars and Ratsey and Lapthorne the Dacron sails.

### 39. FRA 88 Mandragore (F. Camatte, 1939), by *Henri Thibault (France)*

*Mandragore* is spending the winter at the Kervilor yard at La Trinité-sur-Mer. The mast is being completely restored. We're changing the rods. The mast and boom will be refastened, halyards and cables replaced and finally stripping and re-finishing.

We will soon be going to Brittany for the annual maintenance work (checking all the hardware, cleaning, bright work and re-painting of the hull.

We're hoping to participate in the European Championships in June.



40. **FIN 38 Mariana** (G. Jacobsson, 1938), by *Martin Jansson (Finland)*

The members of the new “Team Marianna” are Chris & Gina Bergenheim, Bettina & Kenneth Nyman, Mikael Frisk and me.



41. **GBR 42 Melita** (Fife & Son, 1934), by *David Roberts (U.K.)*

*Melita and the Lock-Keeper's Bicycle*

*Melita* is a William Fife designed 6 Metre commissioned by the ‘King of Whisky’, Mr Ronald Teacher, which he hoped would be selected for the British-American Cup team challenge of 1934. *Melita* was built at the William Fife and Son yard at Fairlie in Scotland and was build number 804. At the same time, another 6 Metre *Saskia II* was also commissioned to be built in the Fife yard for a Mr A.S.L. Young as build number 807, also a contender for the British-American Cup Team. The races were to be held off Oyster Bay in Long Island Sound in September that year, with four boats to be shipped over from Clyde. At the same time, the Royal Northern Yacht Club issued a challenge for the Seawanhaka Cup, to be carried out immediately after the team racing had finished, to be competed for by one of the four Clyde boats selected for the team. *Melita* for heavier winds and *Kyla* for lighter airs were the favoured boats.



*Melita* at left during the 1937 Coronation Regatta (of George V) at Cowes

During the trials in June/July 1934, *Melita* competed with *Vorsa*, *Kyla*, *Susette*, *Volga*, *Saskia II*, *Fintra* and *Piccolo*. *Melita* did well in the trials and was selected for the team along with *Saskia II*, *Kyla* and *Vorsa*. Despite the intensive preparations by the British team, the Americans, ably assisted by *Bob Kat II*, overwhelmingly retained the British-American Cup. *Bob Kat II* beat *Kyla* to retain the Seawanhaka Cup too.

During the 1930's *Melita* went on to be one of the most successful boats in the 6 Metre class on the Clyde and usually topped the prize-winners list. A Melita Trophy is said to exist but little more about this is known

Little is know about the boat during the war except that in 1945, the Teacher's sold *Melita* and it was purchased on Cobh, Cork in the south of Ireland. It is possible that she spent the whole war in Ireland maybe as an 'evacuee' from the Clyde. The new owner George 'Van' Baird sailed *Melita* in Belfast Lough with great success in the Cruiser/Handicap Class. She was kept at the Royal Ulster Yacht Club at Bangor and was often collecting the silverware.

*Melita* was bought in 1951 by Noel Evan-Burns who sailed out of Donaghadee, and subsequently by a Colonel Grove-Raines. At some time a cabin and engine were fitted. *Melita* was sailed across the Irish Sea and whilst passing through the Crinan Canal, the chain drive from the engine's starting handle to the crank became inoperable. It is alleged that one of the crew solved the problem by pinching the chain off the lock-keepers bicycle enabling *Melita* to make a speedy exit from the lock!

*Melita* now back in Scotland, appeared in the Clyde International Regatta in 1972 and little seems to be known about her until she was sold by a Peter Strenger in 1978 to Kent Persson

in Sweden who spent a considerable effort to get her back in racing shape. No longer K42, but S93, *Melita* won the Nordic Championship in 1985 and 1989, came second in the 1988 Europeans and fifth place at the Worlds in Sandhamn in 1995 under the guidance of Kent Persson. The current owner, David Roberts, bought *Melita* in Stockholm in 2003 and took part in local club racing in and around Stockholm, as well as the 2005 World Championships where *Melita* came 13<sup>th</sup> out of 28 classics, now reverting to sail no. GBR 42. During the winter of 2006, *Melita* had a beautiful new deck and a considerable amount of restoration work undertaken in Sweden by the yard Thomas Larsson and Jonas Lorensen, before returning to the UK after an absence of more than thirty years to participate in the 2007 World Cup at Cowes



Melita again at Cowes for the 2007 World Cup (Photo courtesy of Kathy Mansfield - [www.kathymansfieldphotos.com](http://www.kathymansfieldphotos.com))

42. **GER 30 Mena** (C&N, 1946), by *Thomas Kuhnman (Germany)*



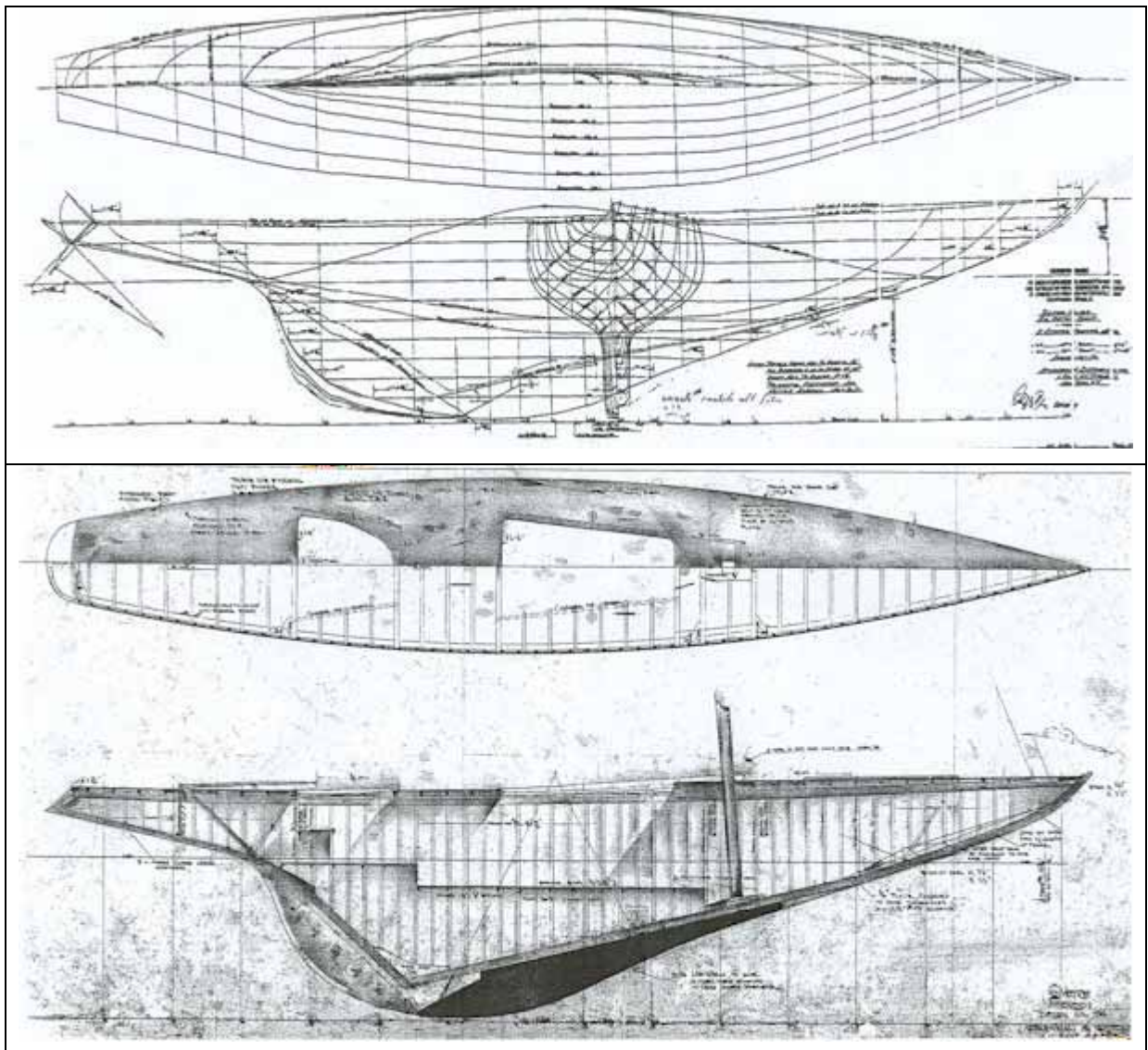
43. **NOR 86 Miranda II** (A.U. Costaguta, 1936), by *Jan Nygaard (U.K.)*

It has been a long way for me to get *Miranda II* back in the condition she is in now.

She is still at Peter Wilson's boat yard, but she will soon be finished. The rigging still has to be done.

44. **KC 24 Mood** (O. Stephens, 1936), by *Duncan Green (Canada)*

*Mood* is completing a structural rebuild, plus new spars/rigging from Danny Klacko in Oakville Canada. He referred to Tim Street's & Pekka Barck's book to select a double spreader rig for her.



I also own the 'P' class *Italia* which is being restored at my home at Kenora. Other boats we have owned: 10M *El Margret* (ex *Rita 5*) gift to Christian X by the people of Denmark in 1937, a Swedish boat, 87' lod, semi - 'J' class *Atlanta* (30T lead ballast and draft of 13'). This last would have been a great restoration project, but she sank alas! The 10M and another 'P' class *Stranger* are awaiting restoration. Wish me luck!

**45. NED 27 Noreg III (J. Anker, 1939), by Marc Heijke (The Netherlands)**

*Noreg III* is the last boat designed and built by the famous Norwegian designer Johan Anker. She was ordered by a consortium that included HRH Crown Prince Olav of Norway, and delivered in 1939. Her name means Norway in Old Norwegian.

Rolf Svinndal took over the boat, won all but one of the races in which she participated, including the One Ton Cup. She was chosen to race for the Seawanhaka Cup on the Clyde in August 1939, but due to "no wind" conditions she lost to *Circe*.

the efforts of F. R. Coudert, Jr., and the State Department that it finally appeared the next spring. Norway had challenged again for the Seawanhaka Cup, held by the Royal Northern Yacht Club, and sent *Noreg III* to the Clyde, a yacht owned by the Rolf Svinndal Syndicate, of which Prince Olaf was a member. She represented the Royal Norwegian Yacht Club and Rolf Svinndal was her helmsman. She was a crack new boat, designed by Anker, and had recently won the One Ton Cup at Hankow. J. Herbert Thom's *Circe*, which had won the Cup off Oyster Bay from *Goose* the year before, was designated as the defender. So two fine yachts, with equally excellent skippers, faced each other. However, lack of wind stretched the series to ten days, as five races were needed to decide the issue. Indeed, at one point abandonment of the match was seriously considered, as war was imminent.

The first race was on August 14th, in light and calm conditions. It is recorded that about one thousand people lined the foreshore and no such scenes had been witnessed since *Britannia-Vigilant* days. Evidently the heavy weather *Circe* made a surprising performance and won by 21 seconds, only one quarter hour within the time limit, after both boats had been becalmed.

The race next day over a triangular course was again won by *Circe* in light conditions. It was a slow one, as indicated by the times:

*Circe*—5 hours 47 minutes 1 second.

*Noreg*—5 hours 54 minutes 58 seconds.

There was a good westerly wind for the third windward-leeward race the day following. *Noreg* took the lead at the start and held it throughout, proving better to windward in both rounds. *Circe* had spinnaker trouble. *Noreg* won by 4 minutes 16 seconds.

It was not until August 21st, five days later, that there was wind enough to complete another race. However, this was a magnificent contest in a fresh northeaster. Both yachts displayed equal speed, both on and off the wind, and the lead alternated. On the last reaching leg, *Circe* was slightly ahead to leeward, but the finish line of the triangular course was not at right angles to the final course and *Noreg* on the inside won by one second in the fast time of 2 hours 20 minutes, in contrast to nearly 6 hours in previous races. Mr. Thom drew official attention to the finish line error, but did not protest. Undoubtedly he should have been the winner.

Again the wind failed and with the score tied at two all the competitors had to wait three days for the final race. A fairly fresh south-east breeze was blowing over the windward-leeward course at the start on August 24th. *Noreg* led by 54 seconds at the first windward mark, but *Circe* turned the tables down wind and was 3 minutes 9 seconds ahead at the end of the first round. The wind held fairly steady on the second turn to windward and *Noreg* went so well that she completely wiped out her opponent's lead and led by 30 seconds at the mark. However, on the last leg the wind failed almost completely and the time limit spectre appeared again. Both boats were several times becalmed, but in the end *Circe* was first to catch a better breeze, overhauled *Noreg*, and romped home the winner by 3 minutes 55 seconds, with only 7 minutes of the time limit unexpired. Perhaps the Scottish crew were lucky, but they had had a bad break with the finish line in the previous race. So like the fine *Coila III* before her, *Circe* successfully defended the Seawanhaka Cup she had originally won. The great trophy was stored away in the Royal Northern Yacht Club and its members went to war. Five years later, I escorted a convoy up the Firth of Clyde one moonlight winter night and thought of the great Six Meter racing there of which I had heard so much. I wish I had raced aboard a Six Meter in that beautiful body of water.

Here ended the great Six Meter era between the two World Wars. After the second conflict, this class racing began again and will be recorded in the third volume of this history. The Seawanhaka Cup was brought home by *Djinn* in 1947 and successfully defended in 1957 by *Goose*, the last Six Meter to race for it. However, in the post-war years exorbitant expense cut down the size of the class. It can certainly be said that between 1921 and 1939 the Seawanhaka Corinthian Yacht Club was the sole sponsor in the United States of international small yacht racing in the Six Meter class. Due to the size of the class and the excellence of the yachts and their crews, this was international racing of a calibre that had never been equalled before or since and may never be again. The terms "Six Meter" and "Seawanhaka" will forever be synonymous.

Excerpt recounting the 1939 running of the Seawanhaka International Cup in 1939 from the John Parkinson, Jr., *The Seawanhaka Corinthian Yacht Club: The Early Twentieth Century 1897-1940* (New York 1965)

The Seawanhaka International Challenge Cup was won in 2005 Southern Yacht Club of New Orleans. To its immense regret, the trophy was lost in Hurricane Katrina shortly thereafter. The club is struggling for resources and time with which to duplicate the cup for another match in 2008.





Five medallions showing: 1.) *US 29 Lanai's* win against *K 20 Coila III* (1925), 2.) *N 27 Noreg's* win against *US 9 Clytie* (1927), 3.) *Bob Kat I I's* win against *K 45 Kyla* (1934), 4 & 5.) *K 55 Circe's* win against *US 81 Goose* (1938) and *N 72 Noreg III* (1939)



Medallion recording *US 81 Goose's* victory over *KC 22 Titia* in 1957 (above) and *Seawanhaka Cup* base (right)



In June 1941, the German occupant confiscated many of the finest yachts in the Oslo fjord. *Noreg III* was assigned to Reichkommisar Josef Terboven. After the war, Crown Prince Olav sailed her and she obtained many good rankings. Olav is entered as the owner in the Royal Norwegian Yacht Club's (RNYC) year-books for 1947 and 1949. In RNYC's 1951 year-book, the merchant Carsten Larsen in Ålesund is entered as the owner.

In the book about the Ålesund Yacht Club's 75th anniversary, it says that she was sold in 1952, but not to whom and to where. It is not to be found in any more RNYC registers. In the late 1950's she was sold to USA under the name *Amigo*. I found her in the 1964 American Lloyd's where she is shown as being in Larchmont Long Island and owned by Garald W. Ford

She was brought back to Europe by a young Danish boatbuilder.

She had a very ugly deckhouse and other bad stuff. Her previous owner bought her in 1997 and immediately removed the deckhouse and started a restoration based on the only remaining drawing.

When my brother Norbert and I bought her, she hadn't been in the water for 4 years.

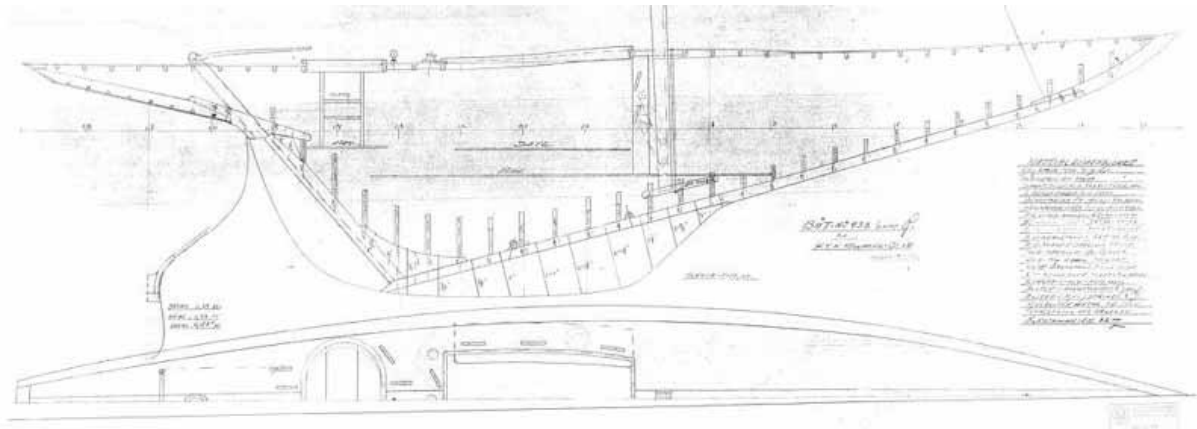


*Noreg III* , with *May Be VI* behind, on 6<sup>th</sup> July 1946 at Hankö (Norway)



Endelig! Det ble l. pr. første gang igjen med Noreg.  
Crown Prince Harald in 1946 at the helm of *Noreg III*

The topsides have been splined.



Her speed potential is enormous, especially against the wind.

**46. FRA 27 Normand III (P. Arbaut, 1928), by *Jean-Claude Danet* (France)**

*Normand III* is celebrating her 80th birthday this year!!!!

We're working with Jérôme Brunet-Moret (Secretary of the UF6mJI) to get her rating certificate updated so that she can participate in the European Championships at La Trinité-sur-Mer in June.



Photo at left: *F 27 Normand III*, *F 43 Philae* and *F 36 Sitaris* pre WWII (Archives of the Deauville Yacht Club)

This summer we'll organise a big birthday celebration for her at her home port of Noirmoutier during the Lancel Classic, the classic boat race series there in August.

For this special year, we're devoting particular attention to her topsides, bright work – as well as her sails, of course!!!

**47. N 22 Oslo II (J. Anker, 1926), by *Peter Giese* (U.S.A.)**

*Oslo II* is moored on Lake Washington, in Seattle, where she will be a fixture when a good breeze is up, especially this spring and summer.

During the restoration work of 2006, she was out of the water several weeks. She took a rather long time to swell up. There was leaking for quite a while but that is now stable and she is as dry as she has ever been.

Overall, the *Oslo II* is in great form and we have the opportunity to clean up many details while she is in Seattle. We still mount a small outboard engine over the port side, on a new bracket.

The Giese family is very proud of *Oslo II* and she will see more of her third Giese generation aboard this year.

**48. FIN 6 Renata (G. Estlander, 1927), by *Kyösti Helin* (Finland)**

In his very intensive and very literary memoirs, *Segel och Sommar*, Henrik Ramsay confirms that Arthur Magnus Pettersson built *Renata*. He mentioned that Pettersson had learned his skills to handle mahogany when he worked at the Russian naval base at Kronstadt, near St Petersburg, where Pettersson built small mahogany skiffs.

*Renata* is still out of the water. Her home port will be at the Merenkävijät ("The Seafarers") yacht club in Helsinki, on the Särkkä isle. The architect, Gustaf Estlander was originally home from Finland, but he moved to Sweden. *Renata* was the last yacht, which he designed for a Finnish owner. He was Minister Henrik Ramsay. She won, among others, the "regatta's best yacht" -award in Sandhamn in 1928.

**49. US 35 Saleema (C.S. Hoyt, 1928), by *Garry Cihak* (U.S.A.)**

SALEEMA is here at the Koehler Kraft boat yard in San Diego. She is in need of some cosmetic work, but structurally she has been epoxied and is sound.

She is also for sale (contact: [garry@koehlerkraft.com](mailto:garry@koehlerkraft.com)).

**50. GBR 30 Selma** (J. Anker, 1926), *by Geoffrey Barker (U.K. & France)*

Selma is for sale (see [www.metreyacht.com/sales/six-metres.htm](http://www.metreyacht.com/sales/six-metres.htm)).



**51. FIN 61 Silène III (T. Holm, 1950), by *Tapani Koskela* (Finland)**

*Silène III* had no sailing last summer due to a missing mast, but the hull was successfully kept in a controlled environment to prevent drying (see picture). Next summer I hope to have her in water and sailing again.

You can see her sailing in Hanko, Finland, in 2004 with her hand painted spinnaker. The flying goat in the figure, designed by artist Jussi Pakkala, refers to Francois Rabelais and the story of Silène in Gargantua: "...in the old days, 'Silènes' were small boxes whose covers were painted with joyful and frivolous figures like crested eagles, satyrs, goslings, horned hares, flying goats and other beautiful paintings".



**52. US 43 Sprig (C. Crane, 1930), by *Greg Stewart* (U.S.A.)**

*Sprig's* 2007 activities included lots of races with other classic boats, but there are no other active 6 meters in San Diego.

The fleet I regularly race with is called the Ancient Mariners Sailing Society which publishes its journal the *Albatross* eight times a year. The fleet includes:

- 1928 A&R 10 meters, Sally & Branta,
- 1947 S&S Pacifica
- 1937 IOD Altair
- 1959 S&S Sloop Brushfire
- Caukins 50 Sabrina
- Navy 44 Yawl Flirt
- K-50 Taroa
- several Pacific Class Yachts



The photo is from the 2007 Yesteryear

Regatta, one of the bigger classic boat events in San Diego. This year I am going to continue preparing for the 2009 World Cup in Newport Rhode Island.

**53. USA 100 St Francis V** (G. Mull, 1973), by *Hank Thayer*

[Editor's note: By mistake we asked Hank Thayer for news about his two Sixes, the other being *Discovery*, without remembering that they were Moderns.]

The Jubilee Book by Tim Street and Pekka Barck is a magnificent contribution to the class, but I'd like to correct a minor error regarding *St Francis V*, viz. it was *St Francis V*, and not *St Francis IV*, that won the first World Cup.

*St Francis V* was the first Gary Mull Six and she won both the Australian-American Challenge and World Cup in her first year.

The photo is of *St Francis V* under construction at the Stone Boatyard in San Francisco in 1973. (Stone also built the first American Six, *Lady Betty*, in 1915.) It appeared on the cover of a prospectus issue of the ISMA News on the occasion of the formation of ISMA.



**54. GBR 66 St. Kitts III** (A.E. Luders, 1937), by *Andrew Thomas (U.K.)*

*St Kitts III* is undergoing some changes with new rig and sails for 2008.

**55. GER 55 Steam** (J. Anker, 1921), by *Thorsten Thelen (Germany.)*

The picture shows *Steam* with her new sails.

In 2007 we restored the underwater painting in April and May.

We sailed only some club race last year.

Also since last year, I have become the owner of a cruising/ racing boat. Having two boats will provide me the leisure to undertake a renewal of *Steam's* deck (flush).

We look forward to racing her again - hopefully against other Second Rule boats.



**56. SWE 60 Stella Polaris** (Z. Westin, 1929), by *Sven Fraenkel* (Sweden)

*Stella Polaris* was repaired last winter after some damage due to hard sailing in strong winds in Sandhamn during the 2005 World Cup. Some planks which had cracked have now been replaced, as well as one winch went loose. The boom was also lengthened.

She is now in top-condition and fit to fight again.



**45. DEN 64 Sun Ray** (A. Laurin, 1939/2005), by *Hans Oen* (U.S.A.)

*Sun Ray* will be at La Trinité in June defending her European Championship.

I will be helming her again and expect to have my EC/WC crew with me.

She is then scheduled to be shipped to the US, possibly as early as right after the EC, to start preparing for 2009 WC in Newport, R.I. For that event, she will be sailed by her owner, Erik T. Larsen.



**59. FRA 20 Tara** (K. Reimers, 1937), by *Jean Brunet-Moret* (France)

Tara underwent work on her ballast keel in 2007.

During the summer, she participated in the Lancel Classic at Noirmoutier, coming in 3<sup>rd</sup> after four races, two of which were very close (very little wind).

**We're preparing for the EC 2008 in June, mainly making sure that her 1937 rating certificate is updated.**



**60. USA 51 Totem** (A.E. Luders, 1930), *by Jesse Smith (U.S.A.)*

Totem has no original plans so we used some high tech gear to find her history. She had stretched sides and hogged ends, so we decided to shoot a cloud of points from a high tech computer laser that we then plugged into CAD drawings.

From there we searched for what was most likely her original shape. With the original boat in the computer, a little historical referencing, and a bit of artistic fairing, we were able to come up with her 1930's self.

It's really amazing. With all of the moulds in place in relation to her keel and back bone, we can remove all the unusable wood leaving a record of the negative space. And then re-plank and rib what is basically air. Anyway, I get a kick out of that.



Totem and I are presently based at Westport , Massachusetts, but we're unsure how long that will be our base. We are maybe headed back to Rhode Island in the next couple of years.

(Editor's note: the restoration Web site is [www.concordiaboats.com/projects\\_totem.html](http://www.concordiaboats.com/projects_totem.html) and Jesse Smith's blog is at [web.mac.com/annice/iWeb/Site%203/Blog/Blog.html](http://web.mac.com/annice/iWeb/Site%203/Blog/Blog.html).)



### 61. ZA ? Vega (?1960), by Gavin Cosgrove (South Africa)

Thanks for your interest in my 6 Metre.

Her name is *Vega* and she was launched in 1960 from the beach in Camps Bay. The workmanship on her is stunning. All the fittings and winches were made by hand in brass.

At the moment, she is moored in Port Owen and I will probably truck her to Hout Bay where I live. The hull has been completely restored. I'm busy restoring the deck and coach roof to the original state. I have also recently found her original "Spruce" mast.

Her hull and deck are mahogany on oak. The hull is painted at the moment but will be sanded down to the wood finish. I'm still not sure what coating/varnish to use, so the paint stays for now to protect her while I'm doing the other work.



I have a file on her development and I will pull out all the interesting details and scan some pictures dating back to the fifties.

### 62. Z 2 Véga (F. Camatte, 1930), by Pierre Frey (Switzerland)

Unfortunately, *Véga* will not be participating at the European Championships.

She is being offered for sale (see listing at the end of this newsletter) in excellent condition and with a new mast. Her sale is definitely not a rush affair.

### 63. KA 2 Venger (Aas, 1946), by Mark Jacobs (Australia)

*Venger* is moored in Watson's Bay in Sydney. She is jointly owned by Mark Jacobs and Nigel Dickson who purchased her in 2003. Her sail number is KA2. Lloyds Register of Yachts Hull Identification Number AU-WWA121528 AE3. She is in good condition and is kept on a swing mooring under full deck covers.

*Venger* was designed by Bjarne Aas Ltd and built by H & J Griffin, at their yard in Bayview on Pittwater, in Sydney in 1946. *Venger* has a transom of Silky Oak reputedly from the old Roseville Church in Sydney and the hull is also stated as being from Catalina planes sourced from Melbourne.



She was originally named *Avenger*, but it was revised to *Venger*.

*Venger* had a key part in the development of early yacht racing in Australia. *Venger* raced actively for more than 20 years in both the Sydney and Melbourne area. *Venger* was built specifically to participate in the Northcote Cup which was named for the Governor General of Australia, as an interstate race commencing in 1908 for yachts not to exceed 7 metre rating. It was converted to 6 metre rating just before World War I.

*Venger*, following an elimination series between *Judith Pihl* and *Cettein*, represented the Sydney Yacht Racing Association (today known as the Yachting Association of New South Wales) in the Northcote Cup of 1947 against *Yeoman*, built by Camper Nicholson to a design by Charles Nicholson. *Venger* lost that race, but held the cup for the Royal Geelong Yacht Club in 1962 and 1963.

Between 1987 and 2002 *Venger* had a major restoration by Mark Holmes. In 2003 Mark Jacobs and Nigel Dickson purchased her. At that time the ring frame was repaired on the port

side and the mast was stepped and painted. The wire rigging was replaced as well as the back stays and runners. There was an overhaul of the outhaul and reefing system, vang and mainsheet blocks. The foredeck hatch cover was rebuilt. In 2004 the interior of the boat was painted and new decking made for the central and stern cockpit. The boom, deck trim plates and oak transom were oiled. In 2006 the unusual wheel helm was returned to a tiller. In 2007 the mast step and winches were replaced.

Both Nigel and Mark are keen to be contacted for any further news on the history and to obtain photos and records. They can be reached at [ndickson@dicksonrothschild.com.au](mailto:ndickson@dicksonrothschild.com.au) and [markj@charterhall.com.au](mailto:markj@charterhall.com.au).

**64. FRA 99 Vert Galant (J. Anker, 1933), by *Laurent Thierry-Mieg and***

We're coming to La Trinité in June. Last year, *Vert Galant* won the series at La Baule.



**65. FRA 86 Vingt Ans Après (F. Camatte, 1939), by *Simone Chevrier Loubier (France)***

Editor's note: Several months ago, we were contacted by Madame Loubier who has since become one of the most enthusiastic supporters of the class in France. We print below some of her recollections about the Sixes in Marseilles in the 1960's, including her boat at the time which is named *Vingt-Ans Après* today:

"In the early 1960's, my brother and I bought *Cabri* (named after her maiden name Chevrier) as my 21st birthday present. The seller was a *restaurateur* who lived near Cannes. She was at a mooring at the Saint-Pierre port of Cannes. To get to her, we found a couple of kids in a rowboat to whom we paid a few cents for the ride.

She was in terrible condition. The deck had been covered with linoleum and there was water practically up to the floorboards.

The return to Marseilles was memorable. From Cannes to Saint-Tropez, Paul Ricard towed us with his motor yacht *Garlaban*. Her considerable speed didn't do any good to *Cabri*'s planking.

The subsequent leg, starting with a passage around Cape Cissé with a strong following wind, was a real test: the spruce mast creaking, the cotton sails parting along the seams and a linoleum deck which had the adhesion of a skating rink. At Bandol, we ran aground. The jib parted at Maire and we were invaded by mosquitoes as we entered the port of Marseilles. We persevered, motivated by our desire to see her berthed at the Société Nautique de Marseille as soon as possible.

Once there, an idyllic period began. There were six Sixes at Marseilles at the time: *Cabri*, Mr Marty's *Dan*, Mr Jacques Mazet's *Marletta* (today Eole II, still at Marseilles), Mr Thuraud's *Namoussa*, Mr E. Julliard's *Woay*, formerly *Vagabonde*, (today *Solitar Nosc*) and Messrs Danglade and Romieux's *Vert Galant*.

There followed a long effort of restoration: purchase of new sails, vacations spent working on her when she would be out of the water, scraping with dizzy spells caused by the caustic soda, and varnishing the interior of the aft section in mid- August on the Mediterranean!!!

The best memories are those of sailing an exceptional boat with extremely precise tacking and finally faultless setting of the spinnaker after hours of practice. The crew was composed uniquely of members of my family: one of my brothers at the helm, I at the running backstays and two cousins on the jib winches. Our average age was 21-22 and we were bent on winning races.



*Cabri IV* on the Med in the 1960's. Below, sunbathing at Frioul Island in the bay of Marseilles

With the recurrent Mistral wind, we lived in constant fear of dismasting. Once the mast did in fact split and we glued it back together. Another time, I remember leaving on a cruise to Porquerolles Island with my brother with a Force 5 Mistral pushing us at 9:00 a.m. when we left. It then rose to Force 6 and even 7 as we zoomed by Sanary-sur-Mer on the way to our destination.”

Editor's note : Today, nearly fifty years later, Madame Loubier still has a boat at the Société Nautique de Marseille.

### Yeoman II restoration

*Yeoman* is coming along very well, a little slow but that's boat restoration.

A lot of work has been put into the accuracy of the project. As you will see from the photos, I did not have much to work with from the original boat. A very rotten and distorted shape with only the fibreglass sheathing keeping it afloat. But together with the boat, the original plans and some old Beken photos, I have been able to solve many problems.



All the re-building is being done to the same timber sizes and construction as the original boat but using modern glues. The only part of the original boat that was salvageable was the stem, main keel, stern post and the transom, and, of course, the lead. All other important components, though rotten, were removed and put aside to be used for templates etc. Much of the hull was then broken up and tipped except for some pieces of the hull up near the bow, which were cleaned and varnished and are now hanging in my study.



The work has progressed to the stage of the last photos where the ribs are being installed. These are laminated in three layers over the moulds and glued and screwed in place. This is producing an extremely strong hull which, I hope, will then not need longitudinal stingers as the original boat did not have these, but it did have a lot of distortion to its shape, especially in the area of the runners. I have also started making up some of the floor timbers starting at the bow. The first 4 are now done and ready to be installed.

Some timbers are now hard to get, so a few changes have been made here.

The keel timbers have been restored out of the original boat and they were Honduras Mahogany of course.

The horn timber up to the stern was 85% rotten and was renewed in Brazilian Mahogany.

The transom is original.

The hull was Honduras Mahogany. This will now be Brazilian Mahogany.

The internal planking is being done in Brazilian Mahogany. The timbers (=frames) are being laminated in three layers, to the same overall dimensions as the original timbers. The timbers were Elm. I am now using Australian Silver Ash which is of a similar colour and weight to Elm and steams and bends extremely well. The Silver Ash is blonde in colour and is one of the best steaming and bending timbers in Australia - may be in the world. With a beautiful straight grain it is very easy to work and plane and holds glue extremely well. The Silver Ash will give me a wonderful contrast with the Mahogany, very similar to the original boat. The Silver Ash is also within my weight considerations for the whole boat.

All the fastenings being used are either Bronze or Copper. So I hope she will be around for another 70 Years.

Another good thing has happened for the cause, as word gets out about this restoration project, different contacts develop. I now have the original winches and the Camper and Nicholson Builders Plaque. This was the best thing of all. As you can see from a couple of the photos, there was nothing much that came with the boat. She had been stripped of most of her equipment.

**67 NED 28 Zephyr (B. Bothén, 1928), by *Kees de Ruiter (The Netherlands)***

*Zephyr* was transported from Norway to The Netherlands in March 2007 and is presently at the yard of Rexwinkel Jachtbouw in Zwijndrecht. Benno Rexwinkel surveyed the boat and made a plan for renovation that focussed mainly on the improvement of the hull. About 35 frames had to be repaired, all old wrought iron. One floor timber had to be replaced by a new stainless steel one and some of the planks and the bow were renewed. A small engine with a foldable propeller was installed as well.



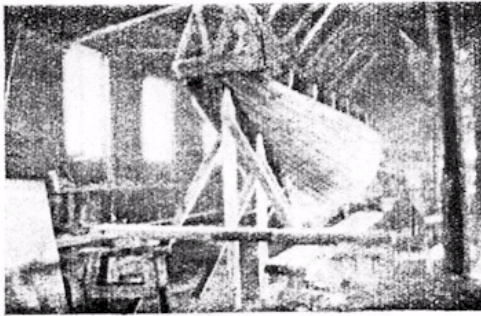
She was designed by Swedish architect Bertil Bothén and built in 1928 at AB Tore Holms Yachtvarv, Gamleby, Sweden as the annual lottery boat of the Royal Swedish Sailing Club. She has a sister ship named *Ramona* (GER 67).

*Zephyr* will be ready in spring of this year for racing in the planned 6mR race in Muiden on 22 May, the first under The Netherlands Int'l Six Metre Association (NISMA) recently founded. However, before any new sail wardrobe will be acquired, we will first extensively work on balancing and trimming of the boat during the coming season.

Editors' note: In the international spirit of the Six Metre class, we end these News from the Fleets with an article in Swedish about *Zephyr* which Kees de Ruitter kindly provided.

## Präktiga bätvinster i G. K. S. S:s lotteri.

Det är åtskilliga präktiga båtar som i vinter blivit byggda för G. K. S. S:s lotteris räkning, nämligen icke mindre än sex stycken. Vi äro nu i tillfälle visa ritningar till två av dessa bätvinster. Samtliga båtar äro i det närmaste färdiga. Tre av dem komma att visas på Svenska Mässan, nämligen kosterbåten, allmagsnipan och trollungen, och i hamnkanalen komma sedan alla sex att pryda upp gatubilden.



Andra vinsten i G. K. S. S:s lotteri under byggnadstiden.

### Andra vinsten, en 6 m. R-jakt.

Denna internationella 6-a, som ritats av ingenjör Bertil Bothén och byggts hos Holms Yachtvarv i Gamleby, har bordläggning av mahogny samt däck av oregonpine. Mast och bom äro av spruce. Båten har följande dimensioner.

Längd över allt	11,34
Längd över vattenlinjen	7,30
Största bredd	2,00
Största djup	1,59
Fribord midskepps	0,67
Segelarea mätt	40,00
Segelarea verklig	51,00

Det är som synes en båt med kraftiga dimensioner, erhållna på bekostnad av den mätta segelarean. För att båten emellertid ej i förhållande till sin storlek och sitt stora deplacement skall få för små segel, har konstruktören sökt utnyttja de möjligheter regeln ger av fri (d. v. s. ej beskattningsbar) segelarea. Båten har för den skull riggats med tvänne höga och ganska smala längsel, varigenom beräknas vinna fördelen av långa dragande lik. För lätt vind skall dessutom begagnas s. k. genuafoclar å båda förstagen.

Däcks- och inombordsanordningarna äro följande:

Styrbrunn, oval med lutande sarger. Brunn för besättningen i två delar inom samma sarg med



# HALLBERGS

H. M. Konungens, H. K. H. Kronprinsens  
Hovjuvelerare

### Sportpriser av Silver och Nysilver

KOSTNADSFÖRSLAG OCH  
RITNINGAR ÖVER SPECIALSAKER

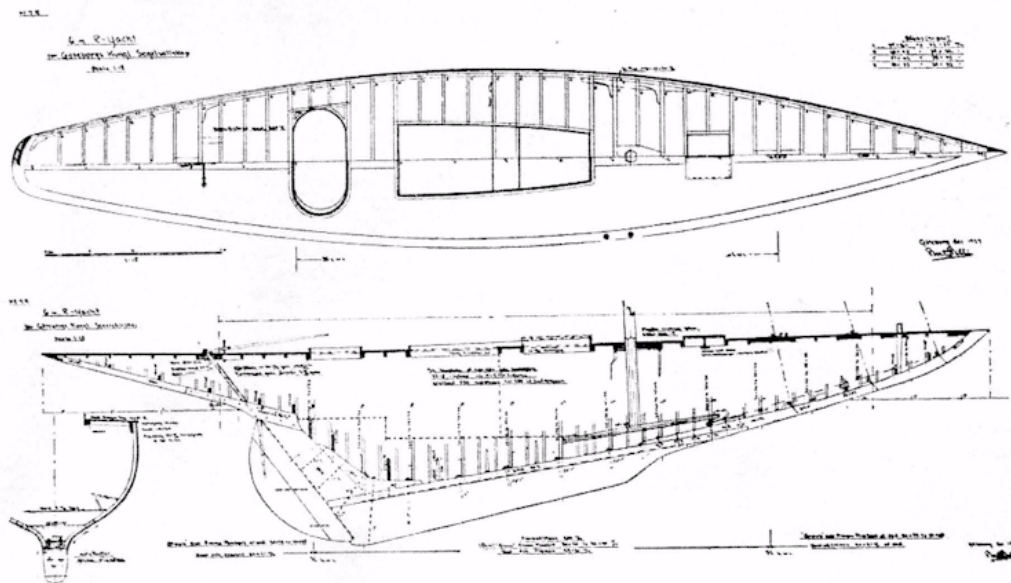
ÖSTRA HAMNGATAN 44  
GÖTEBORG.



10 Maj 1928

SEGLARBLADET N:o 9

95



G. K. S. S:s lotterisera 1928.

den främre delen avskild med en bit däck, skott och löstagbara dörrar jämte löstagbart skylight. Härigenom vinnes att båten blir behöelig i all synnerhet som längs sidorna löpa lätta ribbankar och i den tillslutbara delen anbringas löstagbara fällkojer. Framför masten lucka varifrån segelsättningar och skötsel av försegeln kan utföras.

Konstruktionen avviker förhållandevis mycket från tidigare 6 m. båtar, i all synnerhet genom den längre vattenlinjen, och det härigenom ökade deplacementet, varför det blir av stort intresse få se hur denna Bothéns första 6-a reder sig mot våra andra 6-or. Den torde nog få sin största chance i frisk vind, men en tung båt har som bekant ofta

goda egenskaper även i riktigt lätt vind. Segelveransen har anförtrotts åt segelmakarefirman F. Wilh. Johansson, som lyckades utmärkt med sina segel till 1927 års G. K. S. S:s sexa.

## For Sale

### GER 44 A Y E S H A

Previous names and sail nos.: ex- Ayesha III, K 18 Hadumoth, Wawi, Windspiel XVII

Cabin: yes

Architect: Johan Anker

Builder, location & year: Anker & Jensen Byggnadsår A/S, Vollen (near Asker)  
Norway, 1912

Ballast keel: 2000 kg. (Pb)

Backbone:

Frames:

Planking: Mahogany, double planked

Deck: Teak

Spars: Wood

Sail inventory: Many, including 2 spi, reacher & 3 genoas

Winches: 4

Engine: 2 hp outboard, side-mounted

Electrics: Battery for mastlights & pump

Length overall: 9,61 m.

Waterline length: 5 m.

Beam: m.

Total weight: 1936

Nature & urgency of restoration required: none

Rating & year of latest certificate: 1963 (attached)

Racing history: Lake Constance

Price: 35 ;000 €

Location Lake Constance, Germany

Owner Peter Bartl

e-mail: [peterbartl@gmx.net](mailto:peterbartl@gmx.net)



**Sail plan**



**Cross section**



**Hull profile & cockpit layout**





**For sale**

**K 47 CATHERINE**

Architect: Charles Nicholson

Builder, location & year: Camper & Nicholson, Gosport, U.K. 1935

Price: 15.000 €

Location Saint-Tropez, France

Owner Jean-Denis Sarraquigne

e-mail: [bravade@orange.fr](mailto:bravade@orange.fr)



## For sale

### L 52 Marianne

Previous sail no.: S13

Architect: Tore Holm

Builder, location & year: Holms Yacht Varv , Sweden 1934

Ballast keel: 3000 kg.

Backbone: oak

Floors: galvanised iron

Frames: oak & galvanised iron (1 out of every 3 frames amidships)

Planking: mahogany

Deck: teak

Spar material: aluminium

Sail inventory: racing set: main, 1 and 2 jib, light and spin.

cruising set: main and 2 jib

Winches: 4 old and 4 new.

Electrics / electronics: log

Length overall: 11,45 m.

Moulded depth: 1.6 m.

Beam: 1,8 m.

Total weight: 4000 kg.

Modifications by year: before 70's: cabin, white freeboard

renewed in 70's – 90's: fleet teakdeck, stainless keelbolts, lightened keel, aluminium spar.

renewed in 2000 -> : gunwale board, freeboard paint removed, hull impregnated and varnished, prow stem,

Nature & urgency of restoration required:

Rating & year of latest certificate: 1934

Racing history: 1935 KSSS,s juniregatta 1/2, Sandhamnsregatta 1/9, NJK's 75-years Jubileum 1/22, 1940 Sandhamnsregatta 3/7, 1988 Nordic Championship Winner 1/15

Price: 26 000 euros

Location Turku, Finland

Owner Henri Eklund

e-mail: [henri.eklund@kolumbus.fi](mailto:henri.eklund@kolumbus.fi)



# Rating certificate



**INTERNATIONELLA KAPPS EGLINGSFÖRBUNDET**  
 INTERNATIONAL YACHT RACING UNION

*Internationella regeln för jaktens mätning och mätningstal.*  
*(International Rule for Yacht Measurement and Rating.)*

**MÄTNINGSBEVIS**  
 (CERTIFICATE OF RATING)


STYVTAV AV (ISSUED BY)

**SVENSKA SEGLARFÖRBUNDET.**

Jaktens namn (Name of Yacht) Marianne  
 Rigg (Rig) Decimus Perm. nr 5<sup>13</sup>  
 Ägare (Owner) Jur. konst. Dan Sunden-Cullberg  
 Inregistrerad i (Club) K.S.S.S.  
 Hemmahärade i (Port of Registry) Stockholm  
 Konstrukör (Designer) Ernst Toge-Halm  
 Byggt av (Builder) Leif Yacht Yacht  
 Byggnadsort (Where built) Fårö  
 Byggnadsår (When) 1934  
 Klassificerad i (Classed with) Det Nya Teckas  
 Mätt av (Measured by) C. Gunnar Lund  
 Vid (At) Tynningö och Landhama  
 Mätningen fullbordades (Date Measurements Completed) d. 25 Juni 1934  
 Mätningens bevis upphör att gälla (Date of Expiration) d. 25 Juni 1938  
 (De regler som angående ägares skyldigheter.)

1920 ÅRS FORMEL  $L + 4G + 2d + \sqrt{S} - F = \dots$  m:s mätningstal (Rating)

1933 ÅRS FORMEL  $L + 2d + \sqrt{S} - F = 6$  m:s mätningstal (Rating)

  
 Styvare i Svenska Seglarförbundet  
 (Steward Secretary)

Ann. Överfödig text överskriftn.  
 No. 1. 1.

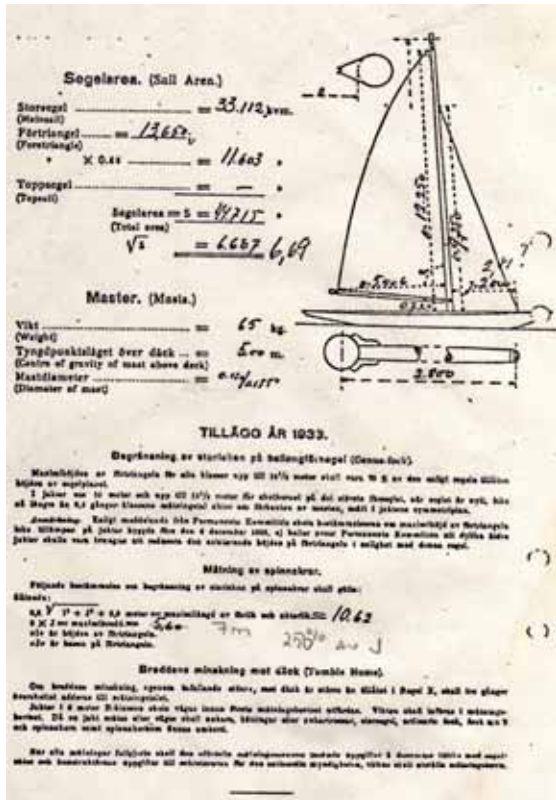
**MÄTT. (Measurements.)**

Längd fram till .....	1.40	11.50	
Överstagets bredd till L <sub>1</sub> .....	2.20	5.80	
Överstagets skär till L <sub>2</sub> .....		7.40	
Ändring för överstagets vrid längd .....	0.85		
Överstagets vrid längd .....	0.40		
Ändring för två skär vid bag O, vid bag .....	0.35		
Tillägg till O, vid bag .....		0.50	
Överstagets vrid skär .....	2.00		
Ändring för två skär vid skären O, vid skären .....	1.15		
Tillägg till O, vid skären .....	0.70		
Barkens längd L <sub>1</sub> .....			8.50
Vindlyg guth. De skär vid till skär .....			
Barkens skär vid O .....			
Ändring för två skär vid O .....			
Överstagets O .....			
I överstagets O .....	1.20		
Ändring för överstagets O till O, bakbord O bakbord .....	1.25		0.00
Ändring för överstagets O till O, styrbord O styrbord .....	1.20		0.00
Tillägg till O, vid bag .....			0.00
Tillägg till O, vid skären .....			0.00
Måttens skär .....			14.10
Måttens skär vid bag O .....	0.85		
Tillägg till O, vid skären O .....	0.85		
Barkens skär .....	1.10		
Ändring (i skär) Frikerd P (max. De tillagge VIII) Barkens .....	1.10		0.20
Skär .....			14.30
Skär med 2.4. 1930 års regl. 2.4. 1930 .....			14.30
Mätningstal (Rating) meter .....			6

Övriga mått, som skola följtas.

Längd fram till .....	1.40	11.50
Överstagets bredd till L <sub>1</sub> .....	2.20	5.80
Överstagets skär till L <sub>2</sub> .....		7.40
Ändring för överstagets vrid längd .....	0.85	
Överstagets vrid längd .....	0.40	
Ändring för två skär vid bag O, vid bag .....	0.35	
Tillägg till O, vid bag .....		0.50
Överstagets vrid skär .....	2.00	
Ändring för två skär vid skären O, vid skären .....	1.15	
Tillägg till O, vid skären .....	0.70	
Barkens längd L <sub>1</sub> .....		8.50
Vindlyg guth. De skär vid till skär .....		
Barkens skär vid O .....		
Ändring för två skär vid O .....		
Överstagets O .....		
I överstagets O .....	1.20	
Ändring för överstagets O till O, bakbord O bakbord .....	1.25	
Ändring för överstagets O till O, styrbord O styrbord .....	1.20	
Tillägg till O, vid bag .....		0.00
Tillägg till O, vid skären .....		0.00
Måttens skär .....		14.10
Måttens skär vid bag O .....	0.85	
Tillägg till O, vid skären O .....	0.85	
Barkens skär .....	1.10	
Ändring (i skär) Frikerd P (max. De tillagge VIII) Barkens .....	1.10	
Skär .....		0.20
Skär med 2.4. 1930 års regl. 2.4. 1930 .....		14.30
Mätningstal (Rating) meter .....		6

Angiv den verkliga ballastens vikt och placering Jager  
 Jaktens vikt full redad 4200  
 Dredningsdjupet för 1.40 1.40  
 Anmärkning angående valutar tillägg vid ballast-  
 viktens avräkning. Angiv om ja eller nej till valutar.  
 Anmärkning angående ballastvikt. Angiv om ja eller  
 nej till ballastviktens avräkning.  
 Har mätningarna erhållit konstruktörens godkännande (An-  
 märkning 1. 8. 4.) Ja  
 Ann. Överfödig text överskriftn.



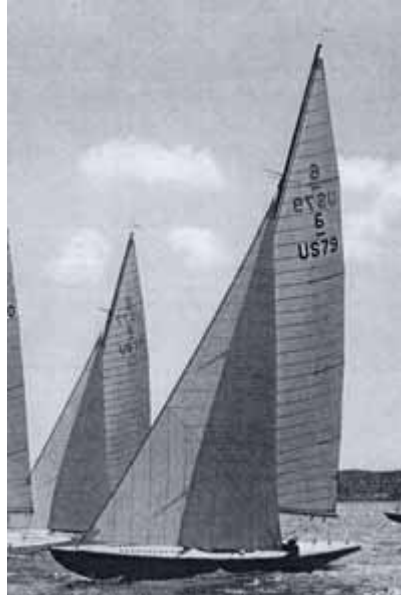
## US 79 STARWAGON

Architect: Herman Whiton  
Builder, location & year: Luders Marine, Stamford, CT 1937  
Ballast keel: Lead (weight unknown)  
Backbone: oak  
Floors: bronze  
Frames: oak  
Planking: copper riveted African mahogany  
Hull covering: 1980's: one layer Dynel & epoxy with splined seams  
Deck: 3 layers: plywood, Dynel & epoxy & teak strips  
Spar material: aluminium mast  
Sail inventory: extensive  
Winches: 4 original S&S under deck, 1 drum missing base & 2 Lewmar self-tailing halyard winches  
Electrics / electronics: Tac-Tic remote knot meter  
Length overall: 11.17 m.  
Beam: 1.84 m.  
Draught: 1.66 m.  
Total weight: 4 330 kg. in 1937  
Cabin: from 1980's  
Engine: 1980 GM Yanmar 6 HP  
Modifications by year: 1970's: deck beams and deck  
Nature & urgency of restoration required: Nothing urgent. She is ready to sail. Perhaps a dozen broken frames.  
Rating & year of latest certificate: 5.99 in 1937 & 1947  
Racing history: 1938-39 Bermuda, 1949 selected for the British American Cup at Cowes and participated in the Scandinavian Gold Cup  
Price: \$25,000 (CAN)  
Location Vancouver, B.C.  
Owner Robert Lawson  
e-mail: briboatlaw@yahoo.com

**Profile**



**Sail plan**



**Deck plan**





# 1947 Rating Certificate

## Certificate of Rating

for  
INTERNATIONAL RULE OF MEASUREMENT  
Yachts of Fourteen and one-half Meters and Under\*

\*See "Note" page 105 North American  
Yacht Racing Union Year Book.

Name of Yacht STAR WAGON  
Rig SLOOP MARCONI 6 METER  
Owner WALBRIDGE STREET ET AL  
Club SEAWANNAKA CORINTHIAN, Y.C.  
Port of Registry NEW YORK  
Designer H. F. WHITON  
Builder LUDERS MARINE CONST. CO.  
Where Built STAMFORD, Conn.  
When 1938  
Measured by B. KARL SHARP  
At STAMFORD, Conn.  
Date Measurements Completed JULY 10, 1947  
Date of Expiration (see Section G, Rule XXIV, relating to obligations of owner) JULY 10, 1949  
RATING 19.68  
B. KARL SHARP Measurer  
YACHT RACING ASSN. OF U.S. Association  
Secretary

## RATING FORMULA

$$\frac{L + 2d + \sqrt{S} - F}{2.37}$$

### MEASUREMENT OF SAIL AREA

#### MARCONI

TRIANGULAR MAINSAILS—The sail shall be set and the area shall be—

(a) LUFF. Measured from top of boom, where in its lowest position it cuts mast, to the top of the sheave, or black band at the after side of the mast. The lowest position of the boom must be indicated by a black band on the mast.

(b) DIAGONAL. Measured from pin of out haul, or slide extended, or black band at outer end of boom, to the nearest point on mast.  
In yachts 12 metres and under:—

$$\text{Area} = \frac{a \cdot b}{2}$$

Height from the deck to where the line of luff of foremost head sail cuts the mast or topmast 31.98 feet.

Fore-side mast to where the line of luff of foremost head sail cuts the bow-sprit of hull 10.73 feet.

Main boom extreme 16.47 feet.

Main boom from aft side of mast to the inner edge of black band at boom end 16.34 feet.

(a) Luff of triangular mainsail 40.25 feet.

(b) Diagonal of triangular mainsail 2.3 feet.

Length of spinnaker boom 10.72 feet.  
including gooseneck from centre fore-and-aft line of mast MEASURED IN ACCORDANCE WITH RULE XXIV, SEC. 2.

#### AREAS OF SAILS

Mainsail 329.24

Foretriangle Total 171.4

" Total  $\times 0.85$  145.67

Topsail —

Sail Area for Rating S = 474.91

$\sqrt{S}$  21.79

### MEASUREMENTS

Overall Length	36.66
Add Overhang Forward to L	4.86
Add Overhang Aft to L	6.67
Subtract Total Overhang	11.53
Measured Length	25.13
Subtract Girth at Bow	2.72
Twice Vertical Height at Bow	1.76
O. at Bow	.76
Add 1 1/2 O. at Bow	1.14
Subtract Girth at Stern	6.54
Twice Vertical Height at Stern	3.76
O. at Stern	2.78
Add 1/2 O. at Stern	.92
CORRECT LENGTH, L.	27.17
Subtract Chain d to d Port	5.77
Chain d to d Port	3.76
d Port	.01
Subtract Skin d to d Starboard	5.78
Chain d to d Starboard	5.77
d Starboard	.01
Add d	.02
2d	.04
$\sqrt{S}$	21.79
Add to find sum of Measurements	49.02
Mean Freeboard Bow O	2.74
Add Mean Freeboard Midship d	3.24
Mean Freeboard Stern O	3.17
Sum of Freeboards	7.15
Subtract (1/2 sum) FREEBOARD, F.	2.38
Total of Measurements	46.64
Divide by 2.37 = Rating =	19.68

### OTHER MEASUREMENTS TO BE RECORDED BY MEASURER

Overall Length	36.66
Add Overhang Forward to L	5.74
Add Overhang Aft to L	6.73
Subtract Total Overhang	12.66
Water Line Length	24.00
Beam Extreme	6.09
Tumble Home	9.00
Actual displacement by weighing	3000#
Approximate weight of, and fore and aft position of ballast inside	300# AT JIB WINCHES
Remarks on condition of water when marks were tested. State Fresh or Salt	SALT Smooth
Remarks on cabin fittings. Stating whether they comply with Schedule	YES
Has Designer's Declaration (para. 4, Section I) been furnished to Measurer?	YES

### DESIGNER'S DECLARATION

The owner must furnish the Measurer with a declaration from the designer or builder, in the following form:—  
Should the declaration prove to be incorrect, the yacht concerned may be disqualified as decided by the National Authority.

Name of Yacht STAR WAGON

(1) I declare that this Yacht's weight is not less than the minimum required by Clause XI of Rule ACTUAL WEIGHT-7800 LBS.

(2) Minimum displacement allowed by Clause XI of Rule is 7530 LBS

(3) Her draught does not exceed 5.44 FT.

(4) Maximum draught allowed by Clause VII of Rule is 5.48 FT.

(5) The highest point of measurement of her sail plan is 42.62 FT

(6) Maximum height allowed by Clause XIII of Rule 42.62 FT

(7) The highest point of Measurement of Foretriangle is 31.98 FT.

(8) Maximum height allowed by Clause XIIIb is 31.98 FT.

Signed by Designer or Builder of the Yacht A. S. Luders, Jr.

Date July 24, 1947



Signed by Owner or his Representative A. S. Luders, Jr. for WALBRIDGE STREET ET AL


















































Date JULY 24, 1947




In 6-Metre Yachts the actual displacement must be ascertained by weighing and must be stated on the Certificate of Rating.


















































#### NOTE



The Designer is not responsible for the correctness of this Declaration after the date thereof, but the Owner must inform the Secretary of any alteration invalidating the same.





























 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing



No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
1	"May Be Not"	?	?	?		Lake Maggiore	IT	?	?	
2	?	?	A&R	?		?	NL	?	?	
3	?	?	?	?		Alexandria	EG	?	?	
4	?	US ?	?	?		Red Sea	EG	?	?	
5	?	US 37	Anker, J	A&J		Columbus, OH	US	?	?	
6	Abu	GBR 32	A&J	A&J		?	UK			
7	Adelheid	GER 41	Rasmussen	A&R		Bodensee/Constance	DE			
8	Aglaja	FIN 1	Wahl	Åbo		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-123048.html">http://www.6mr.fi/DAS/yachts/991227-123048.html</a>
9	Agnes IV	S 7	Ljundberg	Stockholm D		Skien	NO			
10	Aida	GER 59	Aas	Aas		Kiel	DE			
11	Aileen	BRA 1	Hansen, W	K F&S		Rio Yacht Club - Niteroi - Guanabara	BR		?	
12	Airone	ITA 21	?	?		Bay - Rio de Janeiro	IT		?	
13	Alana	US 52	Fife & Son	Fife & Son		Venezia	US			
14	Alexandra	BEL 11	R&C	?		Port-Grimaud	FR			
15	Ali Baba II	FIN 50	Holm, T	Holms Tore		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-115556.html">http://www.6mr.fi/DAS/yachts/981119-115556.html</a>
16	Aloha	SWE 9	Nygren	Eriksson		?	SE		?	
17	Amagao	Z 37	S&S	?		Romanshorn/Bodensee	CH	?	?	
18	Anemone	GBR 4	Schultz	Kolbjornsvik		?	UK		?	
19	Ängel	SWE 80	?	?		?	SE		?	
20	Anitra	FIN 25	Olofsson	Borgå		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-133419.html">http://www.6mr.fi/DAS/yachts/991227-133419.html</a>
21	Anja	FIN 14	Olofsson	N/O		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-132830.html">http://www.6mr.fi/DAS/yachts/991227-132830.html</a>
22	Antinea	FIN 24	Olofsson	R/R		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-210131.html">http://www.6mr.fi/DAS/yachts/981119-210131.html</a>
23	Antiope	Z 13	Holm, T	Holms Tore		Rolle/Léman	CH		?	
24	Anÿe	GER 39	Wilke	Wilke		Bodensee/Constance	DE		?	
25	Aquarius	GER 37	A&R	A&R		?	DE		?	
26	Ara	SWE 72	Aas	?		Stockholm	SE		?	
27	Aramis IX	Z 39	Reimers	Corsier		?	?	?	?	
28	Armeta	FIN 17	Olofsson	Åbo		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-121956.html">http://www.6mr.fi/DAS/yachts/981119-121956.html</a>
29	Artemis	MON 45	Estlander	Norrtälje		Versoix/Léman	CH			
30	Askeladden	N 8	Aas	Aas		Fredrickstad	NO			
31	Asti V	GBR 61	Giles	Sagredo		?	UK		?	
32	Astrée II	SUI 44	Aas	Aas		Versoix/Léman	CH			
33	Attack	FIN-68	Becker, H	?		?	FI			<a href="http://www.6mr.fi/DAS/yachts/1040615-131301.html">http://www.6mr.fi/DAS/yachts/1040615-131301.html</a>
34	Audifax	K 22	Mylne	Bute		Bodensee/Constance	DE		?	
35	Ayesha I	GER 44	Anker, J	A&J		Bodensee/Constance	DE			
36	Azaïs	Z 16	Camatte	Chiesa		?	NL			
37	Bambi	FIN 65	Reimers	Portier		?	FI			<a href="http://www.6mr.fi/DAS/yachts/1020926-124325.html">http://www.6mr.fi/DAS/yachts/1020926-124325.html</a>
38	Barbara	SWE 14	Nygren	Ångholm		?	SE	?	?	
39	Bau Bau	I 58	Baglietto, V	Baglietto, P		Pescallo, Bellagio, Lake Como	IT			
40	Beausobre	Z 15	Mylne	?		Territet/Léman	CH		?	
41	Belanea	SUI 70	Boyd	Robertson		Lac Thun	CH			<a href="http://www.6metre.ch/market_place.html">http://www.6metre.ch/market_place.html</a>
43	Bianca	N 29	Aas	Aas		Gothenburg	SE		?	
44	Bihannic	FRA 90	Camatte	Chiesa		Cannes	FR			
45	Black Rose	US 42	Stephens	Nevins		Lake Champlain, NY	US			
46	Blodoks	US 88	Kristoffersen	Iversen		Port Townsend, WA	US			
47	Bob Kat II	US 54	Stephens	Nevins		Porto San Stefano	IT			
48	Bonite	Z 23	Holm, T	?		Lac Thun	CH		?	
49	Borée II	FIN 63	Aas	Aas		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-214646.html">http://www.6mr.fi/DAS/yachts/981119-214646.html</a>
50	Borgila	FIN 57	Nicholson	Ångholm		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-214003.html">http://www.6mr.fi/DAS/yachts/981119-214003.html</a>
51	Bo-Ve	US 69	Kristoffersen	A&J		Port Huron, MI	US			
52	Briseis	Z 102	Camatte	Chiesa		Bodensee/Constance	CH	?	?	
53	Buri	N 63	Aas	Aas		Kragerø	NO		?	
54	Buzzy II	KC 6	Laurin	Neglinge		Puget Sound	US			
55	Buzzy III	US 97	Stephens	Taylor		Puget Sound	US			
56	Ca Va	KC 12	Wedellsborg	Nordbjaerg		Vancouver, BC	CA			











































 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing



No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
57	Cailin	F 23				Dungarvan Harbour Sailing Club	IE			
58	Caprice	GBR 48	McGruer, J	McGruer, J		?	UK			
59	Caramba	H 21	Holm, T	Önnereds		?	NL		?	
60	Caresse	FIN 2	Westin	Åbo		Bodensee	DE			
61	Carin II	CAN 8	Kyntzell	Wilenius		Lallows, Cowes	CA			
62	Carmela	SWE 15	Estlander	Plyms		?	SE	?	?	
63	Catherine	K 47	C&N	C&N		Saint-Tropez	FR			
64	Challenge	US 65	Luders	Luders		Puget Sound	US			
65	Cherokee	US 53	Stephens	IYRS		Narragansett Bay, RI	US			
66	Cinq Août	F 26	Guédon	Bonnin		Toulon	FR		?	
67	Ciocca II	I 64	S&S	Baglietto, P		Pen Pol, Cornwall	UK			<a href="http://www.metreyacht.com/sales/cioccai.htm">http://www.metreyacht.com/sales/cioccai.htm</a>
68	Circe	US 74	Luders	Luders		Cornwall	UK		?	
69	Clarity	D 22	Fife, W&R	Thornycroft		Rhode Island	US			
70	Clyde	FRA 109	Fife, W	?		Loctudy	FR			
71	Clymène II	B 66	Aas	Aas		Saint-Tropez	FR			
72	Clytie II	US 33	Crane	Nevins		Massachusetts	US	?		
73	Colibri	E 22	A&G	Hève		Valencia	ES			<a href="http://www.hollerboats.com/d1/details.asp?lan=esp&amp;ID=130&amp;where=propiedades.tipo=2">http://www.hollerboats.com/d1/details.asp?lan=esp&amp;ID=130&amp;where=propiedades.tipo=2</a>
74	Colibri	L 62	?	?		Risør	NO			
75	Cresta		C&N	C&N		Piraeus	GR	?	?	
76	Cupidon-Fou	F 38	Camatte	Bonnin		La Rochelle	FR	?		
77	Cutty	D 42	Witt & Wedell	Nordbjaerg		Danmarks Museum for Lystsejlad	DK			
78	Dan	DEN 43	Aas	Aas		Lake Como	IT		?	
79	Dana	POL 4	Giles	Giles		?	PL			
80	Diamant	SWE 1	Andersson, Albin	Hästholsvarfvet		?	SE		?	
81	Diana	FRA 40	Robert	Holmens		Baud	FR		?	
82	Dimple	GER 77	Anker, J	A&R		?	DE		?	
83	Dinorah II	FRA 65	Camatte	Bonnin		Saint Didier s/Charalonne	FR			
84	Dix Août	FRA 111	Aas	Aas		Arradon	FR			
85	Djinn	FIN 67/ US 80	Stephens	Nevins		?	FI			<a href="http://www.6mr.fi/DAS/yachts/1030918-110536.html">http://www.6mr.fi/DAS/yachts/1030918-110536.html</a>
86	Dodo	US 89	Aas	Nordbjaerg		Port Huron, MI	US	?	?	
87	Dorothee	GER 2	Rasmussen	A&R		?	DE		?	
88	Duet	GBR 15	Glen-Coats	McGruer		Cowes	UK			
89	Dutch Naan	D 1	Broenden	?		?	NL	?	?	
90	Edelweiss II	BEL 17	Hope	Maynard		?	BE			
91	Edith	NED 8	Aas	Soon		?	NL		?	
92	Elfe	FRA 77	Camatte	Bonnin		Noirmoutier	FR			
93	Elinore	FIN 35	Olofsson	Hietalahti		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-210812.html">http://www.6mr.fi/DAS/yachts/981119-210812.html</a>
94	Elizabeth IX	N 42	Aas	Aas		?	DE			
95	Elizabeth X	KC 9	Aas	Aas		Mamaroneck or Larchmont, NY.	US			
96	Elizabeth VIII	N 39	Robert	Holmens		Holmen Yachtverft	NO			
97	Emden	GER 7	Hacht	Hacht-Werft		?	DE		?	
98	Emzia	FIN 64	Anker, J	A&J		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-215040.html">http://www.6mr.fi/DAS/yachts/981119-215040.html</a>
99	Eole	FRA 79	Camatte	Chiesa		Ile d'Yeu	FR			
100	Eole II	FRA 85	Camatte	Monti		Marseille	FR			
101	Erica	GBR 57	Nicholson	C&N		?	UK			
102	Fågel Blå	SWE 6	Holm, T	Holm		Stockholm	SE			
103	Fandango	FIN 40	Holm, T	Wilenius		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-123135.html">http://www.6mr.fi/DAS/yachts/981119-123135.html</a>
104	Farfadet	SUI 67	Holm, T	Holms Yachtvarv		Prangins/Léma	CH		?	
105	Finn	SWE 24	Stenbäck	Åbo		?	SE		?	
106	Fintra	K 7	Fife & Son	Fife & Son		?	DE			<a href="http://www.fintra.de">http://www.fintra.de</a>
107	Fintra II	KC 16	Fife & Son	Fife & Son		Vancouver, BC	CA			
108	Finvola	GBR 19	Fife & Son	Fife & Son		?	UK			
109	Fiona	NED 25	Fife, W	Fife & Son		?	NL			
110	Fissa	FRA 120	Baglietto, P	Baglietto, P		Cassis	FR			
111	Flapper	N 71	Jensen, C	A&J		Maine	US			
112	Flush	SWE 25	Becker, H	Rödösund		Stockholm	SE			









































 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing



No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
113	Fly	D 29	Anker, J	Nielsen, B	?	Kalmersund	SE		?	
114	Fokus III	US 90	Aas	Aas		Poulsbo, WA	US			
115	Freya	OE 13	Arbaut	Hève	?	Bodensee/Constance	CH	?	?	
116	Freya	SUI 11	Camatte	Chiesa	?	Lac Thun	CH			
117	Fridolin	FIN 12	Holm, T	Holms Tore	?		FI			<a href="http://www.6mr.fi/DAS/yachts/1040210-203431.html">http://www.6mr.fi/DAS/yachts/1040210-203431.html</a>
118	Fromista	FIN 56	Estlander	Kungsörs	?		FI			<a href="http://www.6mr.fi/DAS/yachts/991227-140855.html">http://www.6mr.fi/DAS/yachts/991227-140855.html</a>
119	Fun	FRA 119	Stephens	Nevins		Dinard	FR			
120	Galejan	SWE 23		Bothén ?	?		SE			
121	Gallant	KC 10	Laurin	Plyms		Sidney, B.C.	CA			
122	Gaviota	GER 3	Neesen	Travag			DE			
123	Gefion III	K 37	Anker, J	A&J		Dellach am Wörthersee	AT			
124	Georgina	KA 4	Fife, W&R	Tyson		Cammeray, Sydney	AU			
125	Goose	US 81	Stephens	Nevins		Puget Sound	US			<a href="http://www.gooseus81.com">http://www.gooseus81.com</a>
126	Grebe	US 3	Gardner	Nevins	?	New York	US	?	?	
127	Guignol V	Z 1	Camatte	?	?	Choiseul/Léman	CH	?	?	
128	Guildenfluen	D 4	Anker, J	?	?	Ebeltoft, Jylland	DK	?	?	
129	Gulldisken	SWE 4		Holmström		Arradon	FR			<a href="http://quip56.over-blog.com/article-7223165-6.html">http://quip56.over-blog.com/article-7223165-6.html</a>
130	Gulldisken (ruffad)	SWE 49	Holmström	Holmström			SE			
131	Gun II	SWE 42	Westin	?	?		SE			
132	Gunda Marina	FIN 28	Olofsson	Urho	?		FI			<a href="http://www.6mr.fi/DAS/yachts/991227-133557.html">http://www.6mr.fi/DAS/yachts/991227-133557.html</a>
133	Gybo	?	Laws	Burnham		Flushing	NL	?	?	
134	Hakahala	D 48	Reimers	K Y&M		Genemuiden	NL			
135	Hamburg	G 1	Rasmussen	A&R	?		DE		?	
136	Hanko III	US 96	Aas	Aas	?	Puget Sound	US			
137	Houri	GBR 3	Payne	McDonald		Usk, Monmouthshire	UK			
138	Høvding	N 78	Berg	Borge			NO		?	
139	Hungária	HUN 1	Estlander	Ibusz		Lake Balaton	HU			
140	Hurricane	KA 16	Barber	?	?		AU	?	?	
141	Hypnos	GER 43	Rasmussen	A&R	?		DE		?	
142	Ian	FIN 59	Estlander	Arendals			FI			<a href="http://www.6mr.fi/DAS/yachts/981119-130354.html">http://www.6mr.fi/DAS/yachts/981119-130354.html</a>
143	Indian Scout	US 66	Whiton	Luders	?	Port Townsend, WA	US			
144	Ingegerd	SWE 22	Estlander	Arendals	?	Lux	SE		?	
145	Irene	US 90	Aas	Aas		Port Huron, MI	US			
146	Irma	FIN 3	Slotte	Wilenius			FI			<a href="http://www.6mr.fi/DAS/yachts/981119-123805.html">http://www.6mr.fi/DAS/yachts/981119-123805.html</a>
147	Irimi V	G 37	Rasmussen	A&R			NL			
148	Izenah II	FRA 60	Bréguet	Macario		Vannes	FR		?	
149	Jeux d'Eau	Z 19	Camatte			Crans/Léman	CH		?	
	Jill	US 56	Stephens	Nevins		Rockport, ME	US			<a href="http://web.mac.com/marthafaycoolidge/iWeb/Site/Welcome.html">http://web.mac.com/marthafaycoolidge/iWeb/Site/Welcome.html</a> & <a href="http://www.rockportmarine.com">http://www.rockportmarine.com</a>
150										
151	Jo	GBR 19	Anker, J	A&J			UK			
152	Joanna	F 75	Drewitz	Buchholz		Saint-Tropez	FR			<a href="http://www.joanna.fr">http://www.joanna.fr</a>
153	Johan of Rhu	KC 17	McGruer, J	McGruer, J		Vancouver, B.C.	CA			
154	Joker	D 53	W-W	?	?		?	?	?	
155	Jolanda III	FIN 39	Wahl	Lemposaaari	?	Helsinki	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-211628.html">http://www.6mr.fi/DAS/yachts/981119-211628.html</a>
156	Joy	FIN 36	Holm, T	Wilenius	?	Lovisa	FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-211104.html">http://www.6mr.fi/DAS/yachts/981119-211104.html</a>
157	Junge	KC 21	W-W	Nordbjaerg		Ontario	CA			
158	Juno	KA 15	Fife, W&R	?			AU	?	?	
159	Juno	KC 15	C&N	C&N	?	Nova Scotia	CA	?	?	
160	Kerttu	FIN 5	Westin	Åbo	?		FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-205214.html">http://www.6mr.fi/DAS/yachts/981119-205214.html</a>
161	Klara Stjärna	FIN 21	Stenbäck	Åbo	?		FI			<a href="http://www.6mr.fi/DAS/yachts/981119-205810.html">http://www.6mr.fi/DAS/yachts/981119-205810.html</a>
162	Kobold XV	GER 28	Lehmann	Lehmann	?		DE		?	
163	Korrigan	Z 40	Camatte	Chiesa		SNG/Léman	CH			
164	Kyla	K 45	Mylne	Mylne		Conservatoire Maritime du Havre opposite Caernarfon Castle, North Wales	FR			<a href="http://www.conservatoire-maritime.com/">http://www.conservatoire-maritime.com/</a> click on "Médiathèque" and "Blue Monday"
165	Kyria	K 71	C&N	C&N	?		UK		?	































 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing

No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
166	La Bandera	FRA 14	Aas	Aas		?	FR			
167	La Joyeuse	C 17	?	?		Léman	CH	?	?	
168	Lady Day	DEN 58	Anker, J	A&J		Copenhagen	DK			
169	Le Huit	Z 8	Sundén	Arendals		Léman	CH	?	?	
170	Leila	L 42	Fife, W	Fife & Son		Crinan, Argyll and Bute, Scotland	UK			
171	Leonor	A 1	Baglietto, V	Baglietto, P		?	AR			<a href="http://www.6metre.com.ar/">http://www.6metre.com.ar/</a>
172	Lillevi	GER 68	Westin	Åbo		?	DE			
173	Lilo-Reet II	FIN 31	Holm, T	Kül		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-135438.html">http://www.6mr.fi/DAS/yachts/991227-135438.html</a>
174	Lisbeth V	SWE 136	Anker, J	A&J		Orust	SE			
175	Llanoria	US 83	Stephens	Nevins		Puget Sound	US			
176	Lucie II	US 55	Crane	Nevins		Maine	US			
177	Lully II	N 61	Aas	Aas		Puget Sound	US	?	?	
178	Lulu	US 72	Stephens	Nevins		Poulsbo, Puget Sound	US			
179	Luv	GER 25	Wilke	Wilke		?	DE		?	
180	Madcap	US 23	Hoyt, F	Nevins		Newport, RI	US			
181	Maida	GBR 1	Stephen, J	Stephen, J		Solent	UK			
182	Maj-Lis	FIN 29	Olofsson	Bleholm		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/990427-081707.html">http://www.6mr.fi/DAS/yachts/990427-081707.html</a>
183	Mandragore	FRA 88	Camatte	Bonnin		La Trinité-sur-Mer	FR			
184	Marabu	GER 21	Holm, T	Holms Tore		Bodensee/Constance	DE		?	
						Rio Yacht Club - Niteroi - Guanabara				
185	Marga	BRA 2	Westin	Åbo		Bay - Rio de Janeiro	BR			
186	Margaux	?	Aas	Aas		Bordeaux	FR			
187	Mariana	FIN 38	Jacobsson	Wilenius		?	FI	?		<a href="http://www.6mr.fi/DAS/6mrclass/files/fin38_race.htm">http://www.6mr.fi/DAS/6mrclass/files/fin38_race.htm</a>
188	Mariane	Z 42	Aas	Aas		Bodensee/Constance	CH	?	?	
189	Marianne	FIN 52	Holm, T	Holm		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-212706.html">http://www.6mr.fi/DAS/yachts/981119-212706.html</a>
190	Marianne IV	G 9	Aas	Aas		Dellach am Wörthersee	AT	?	?	
191	Marquise	AUT 7	Sundén	?		Bodensee/Constance	CH			
192	May Be IV	FIN 53	Holm, T	A&B		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-213345.html">http://www.6mr.fi/DAS/yachts/981119-213345.html</a>
193	May Be VI	FIN 51	Holm, T	Holms Tore		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-124652.html">http://www.6mr.fi/DAS/yachts/981119-124652.html</a>
194	May Be VII	US 87	Stephens	Holms Tore		Puget Sound	US			
195	May Be VIII	FRA 105	Holm, T	Holm		Arradon	FR		?	
196	Mebi	ITA 30	Giovannelli	Baglietto, P		Lake Maggiore.	IT		?	
197	Mecara	Z 45	S&S	Corsier		Romanshorn/Bodensee	CH		?	
198	Melita	GBR 42	Fife & Son	Fife & Son		?	SE			
199	Mellum	GER 22	Burmester	Burmester		?	DE		?	
200	Mena	GER 30	C&N	C&N		?	DE			
201	Merenneito I	FIN 22	Westin	Hanko		Woodwind Yachts, Ontario, CA	FI		?	<a href="http://www.6mr.fi/DAS/yachts/1050420-153200.html">http://www.6mr.fi/DAS/yachts/1050420-153200.html</a>
202	Merenneito II	SWE 21	Westin	Åbo		?	SE			
203	Midinette	Z 24	C&G	?		Prangins/Léman	CH		?	
204	Miranda II	NOR 86	Costaguta	Costaguta		Pen Pol, Cornwall	UK			
205	Mizar III	ITA 63	S&S	Sangermani		?	IT		?	
206	Monsoon	GBR 34	Blake	United		?	UK			
207	Monya	FIN 7	Westin	Åbo		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/991227-132408.html">http://www.6mr.fi/DAS/yachts/991227-132408.html</a>
208	Mood	KC 24	Stephens	Nevins		Port Credit, Ontario	CA			
209	Mosquito	L 8	A&J	A&J		Risør	NO			
210	Musette	L 14	Hope	H,H		Ile-aux-Moines	FR		?	
211	Nada	?	Fife & Son	Fife & Son		?	AG		?	
212	Namoussa	FRA 59	B&A	Aas		Auray	FR		?	
213	Nancy	GBR 28	Stephens	Nevins		?	UK			
214	Neerlandia	?	Lentsch	Lentsch		Ouchy/Léman	CH	?	?	
215	Nisidia	FRA 33	Holm, T	Trahamssen		Noirmoutier	FR			
216	Nitra	FRA 72	Guédon	Bonnin		?	FR		?	
217	Nixflu	FIN 18	Fife, W	Abrahamsson		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-133025.html">http://www.6mr.fi/DAS/yachts/991227-133025.html</a>
218	Noa	K 49	McGruer, J	McGruer, J		Puget Sound	US		?	
219	Noreg III	NED 27	Anker, J	A&J		?	NL			
220	Noresca	K 9	Aas	Aas		?	FR	?	?	

 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing

No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
221	Normand III	FRA 27	Arbaut	Hève		Noirmoutier	FR			
222	Norna	N 31	Anker, J	A&J		Oslo	NO		?	
223	Norna II	N 43	Anker, J	A&J		Grimstad	NO		?	
224	Norna IV	S 82	Anker, J	A&J		?	NL			
225	Oberspree	GER 19	Martens	Rambeck		Potsdamer Yacht Club, Wannsee	DE		?	
226	Off Course	FIN 60	Becker, H	?		?	FI			<a href="http://www.offcourse.fi">http://www.offcourse.fi</a>
227	Ondine	US 94	S-W	Aas		Port Huron Yacht Club, MI	US		?	
228	Oslo	US 37	Anker, J	A&J		Cleveland, OH	US	?	?	
229	Oslo II	N 22	Anker, J	A&J		Puget Sound	US			
230	Oui Oui	D 15	Anker, J	A&J		?	DK		?	
231	Phönix	Z 26	Myline	Bute	?	Arbon/Bodensee/Constance	CH	?	?	
232	Piccolo	NED 19	Fife & Son	Fife & Son		Geervliet	NL			
233	Pink Floyd	OE 45	Reimers	?		Lindau/Bodensee	CH	?	?	
234	Puckie	FIN 19	Holm, Y	Norrälja	?	?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/991227-133219.html">http://www.6mr.fi/DAS/yachts/991227-133219.html</a>
235	Rahahu	K 34	Giles	MG&M		Pörtsach am Wörthersee	AT	?	?	
236	Raili	FIN 30	Stenbäck	Åbo		?	FI		?	<a href="http://www.6mr.fi/DAS/yachts/981119-210536.html">http://www.6mr.fi/DAS/yachts/981119-210536.html</a>
237	Ramona	GER 67	Bothén	Holms Tore	?	?	DE		?	
238	Ran VIII	DEN 3	Anker, J	A&R		?	CH		?	
239	Räven	SWE 37	Estlander	Abrahamsson		Arradon	FR			
240	Regina	SWE 36	Estlander	?		?	SE		?	
241	Renata	FIN 6	Estlander	Pettersson, A		Merenkävijät Y.C., Helsinki	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-131931.html">http://www.6mr.fi/DAS/yachts/991227-131931.html</a>
242	Rita	GER 8	Liljegren	Ångholm	?	?	DE		?	
243	Royal Thames	FRA 108	Boyd	Woodnutts		Arradon	FR			
244	Saga	US 73	Aas	Aas	?	Puget Sound	US			
245	Saga	Z 43	Iversen	?		Nyon/Léman	CH	?	?	
246	Saimaa	SWE 56	Stenbäck	Åbo	?	?	SE	?	?	
247	Sakie	US 1	B&P	Lawley		Martha's Vineyard, MA	US		?	
248	Salao	?	?	?		Palma, Mallorca	ES	?	?	
249	Saleema	US 35	Hoyt, C.	Nevins		San Diego, CA	US			
250	Sally	H 10	Lentsch	Lentsch		?	NL		?	
251	Sara af Hangö	FIN 49	Kyntzell	Granstrom		?	FI			<a href="http://www.6mr.fi/DAS/yachts/1060622-092807.html">http://www.6mr.fi/DAS/yachts/1060622-092807.html</a>
252	Saskia II	KC 19	Fife, W&R	Fife & Son		Vancouver, BC	CA			
253	Saskia of Rhu	K 14	Myline	Bute		Saint-Tropez	FR			
254	Scout					?	NZ			
255	Sea Lark	US 63	Stephens	Nevins	?	Chesapeake Bay	US	?	?	
256	Selma	GBR 30	Anker, J	A&J		Bono	FR			<a href="http://www.metreyacht.com/sales/six-metres.htm">http://www.metreyacht.com/sales/six-metres.htm</a>
257	Senoia	B 10	Nicholson	C&N		?	BE	?	?	
258	She	ITA 32	Baglietto, P	?		?	IT		?	
259	Sheila	GBR 25	Burgess	Herreshoff Mfg	?	Saltash, Cornwall	UK		?	
260	Silène III	FIN 61	Holm, T	Holms Tore		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-125927.html">http://www.6mr.fi/DAS/yachts/981119-125927.html</a>
261	Silk	US 63	Stephens	Nevins	?	Portland, ME	US		?	
262	Silvervingen	SWE 31	Sundén	Hägerstens		Nyköping	SE		?	
263	Sioma II	K 17	Anker, J	A&J		Cowes	UK		?	
264	Sira	KC 26	Reimers	Nordbjaerg		Toronto	CA	?	?	
265	Sjo-Ro	KA 3	Fife, W&R	Coverdale		Sydney	AU			
266	Skade	S 1	Herlin	Ångholmens	?	Kotka	FI		?	
267	Skamander	GER 16	Wilke	Wilke	?	?	DE		?	
268	Sleipner II	GER 17	Rasmussen	A&R	?	Schlei	DE		?	
269	Sleipner IV	GER 38	Rasmussen	A&R	?	Eckernförde	DE		?	
270	Solenta	US 78	Nicholson	C&N	?	Port Huron, MI	US		?	
271	Solitar Nosc	FRA 12	Reimers	Corsier		Antibes	FR		?	
272	Sonoma	K 15	Watson	McGruer & Son		Poole, Dorset	UK			
273	Sprig	US 43	Crane	Nevins	sr	San Diego	US			
274	Spyr	Z 17	Camatte	?		Yverdon/Neuchâtel	CH		?	
275	St Amour	GBR 91	Coppinex	Corsier	?	?	UK			
276	St. Kitts III	GBR 66	Luders	?		Fowey	UK			<a href="http://www.metreyacht.com/sales/six-metres.htm">http://www.metreyacht.com/sales/six-metres.htm</a>

 = Whereabouts unknown = Out of the water = Undergoing restoration = Sailing / racing

No.	Most recent known name of the boat	Sail No.	Architect short name	Builder short name	Status	Home Port	Home Country	Owner		Web site
								Name	e-mail	
277	Star VI	D 20	Dahlström	K Y&M		Dellach am Wörthersee	AT			
278	Star Wagon	US 79	Whiton	Luders		Ladysmith, BC	CA			
279	Steam	GER 15	Anker, J	A&R		?	DE			
280	Stella Polaris	SWE 60	Westin	Åbo		Stockholm	SE			
281	Stromer	AUT 48	Stephens	Corsier	?	Bodensee/Constance	CH		?	
282	Sun Ray	DEN 64	Laurin	Jensen, J		?	DK			
283	Sunshine	K 5	Fife & Son	Fife & Son		SNG/Léman	CH			
284	Susette	K 25	Fife & Son	McGruer & Son	ow	Co. Cork	IE		?	
285	Suzan	GER 18	Rasmussen	A&R	?	Bodensee/Constance	DE		?	
286	Syce	US 15	Alden	Lawley		Stamford, Connecticut	US		?	
287	Taiaut	G 38	Reimers	Corsier	?	Kreuzlingen/Bodensee	CH		?	
288	Talizmán	M 2	Stephens	Hungarian		Lake Balton	HU			
289	Tao	US 64	S-W	Luders		Port Townsend, WA	US			
290	Tara	FRA 20	Reimers	Trahamssen		Noirmoutier	FR			
291	Thistle	K 72	Boyd	Robertson	?	Southampton	UK			
292	Tieger	ITA 48	Aas	Aas	?	?	IT		?	
293	Tien Hoa	US 5	Mower	Nevins	?	Long Island, NY	US	?	?	
294	Titia	KC 22	Boyd	Woodnutts		Pen Pol, Cornwall	UK			<a href="http://www.metreyacht.com/sales/six-metres.htm">http://www.metreyacht.com/sales/six-metres.htm</a>
295	Toogooloo II	KA 5	Fife, W&R	Savage		?	AU			
296	Topp	SWE 38	Holm, Y	Holms Häst	?	?	SE			
297	Totem	USA 51	Luders	Luders		Wesport, MA	US	?		<a href="http://web.mac.com/annice/iWeb/Site%203/Blog/Blog.html">http://web.mac.com/annice/iWeb/Site%203/Blog/Blog.html</a>
298	Toy	FIN 44	Kyntzell	Wilenius		?	FI			<a href="http://www.6mr.fi/DAS/yachts/981119-211941.html">http://www.6mr.fi/DAS/yachts/981119-211941.html</a>
299	Trix	GER 1	Hacht	Hacht-Werft	?	?	DE		?	
300	Twins V	ITA 60	Baglietto, P	Baglietto, P	?	?	IT		?	
301	Undine	G 26	Rasmussen	?	?	Bodensee/Constance	CH	?	?	
302	Unita	L 46	Kyntzell	Wilenius		Larvik	NO		?	
303	Valama	FRA 28	Nicholson	Nicholson		Dinard	FR		?	
304	Valdai	GBR 40	Mylne	Mylne		Aldeburgh Boatyard, Suffolk	UK		?	
305	Vanity	SWE 3	Holm, T	Holm, K	?	Lux	SE		?	
306	Västanfläkt	GER 27	Holm, T	Holm		?	DE		?	<a href="http://www.6mr-klassiker.de/en/SY_Vastanflakt/sy_vastanflakt.html">http://www.6mr-klassiker.de/en/SY_Vastanflakt/sy_vastanflakt.html</a>
307	Vega	?	?	?		Port Owen, South Africa	ZA			
308	Véga	Z 2	Camatte	Chiesa		Ouchy/Léman	CH			
309	Venger	KA 2	Aas	G&G		Sydney	AU			
310	Vera	SWE 16	Holm, T	Holm	?	?	SE	?	?	
311	Vert Galant	FRA 99	Anker, J	A&J		Noirmoutier	FR			
312	Victoria	N 51	Anker, J	A&J		?	UK			<a href="http://web.mac.com/belvisi/Belvos_Sailing/Sailing.html">http://web.mac.com/belvisi/Belvos_Sailing/Sailing.html</a>
313	Vingt Ans Après	FRA 86	Camatte	Bonnin		La Trinité-sur-Mer	FR			
314	Vinnia	US 91	Jensen, C	Soon	?	Port Huron	US	?	?	
315	Violetta II	ITA 66	Baglietto, P	Baglietto, P	?	?	IT		?	
316	Vi-Vi	N 30	Aas	Soon		Orcas Island, Puget Sound	US			
317	Why Not	US 99	Holm, T	Holm		Port Huron, MI	US			
318	Wire	FIN 43	Kyntzell	?		?	FI			<a href="http://www.6mr.fi/DAS/yachts/991227-140406.html">http://www.6mr.fi/DAS/yachts/991227-140406.html</a>
319	Yam-Sing	N 83	Aas	Aas	?	Stockholm	SE			
320	Yeoman II	KA 1	C&N	C&N		?	AU			
321	Ylliam VIII	Z 42	Aas	Aas		Berlin	DE		?	
322	Zenith	K 8	Fife & Son	Fife & Son	?	Penrhyn, Cornwall	UK		?	
323	Zephyr	N 88	Bothén	Holms Tore		?	NL		?	
324	Zita	?	Weber	Hestehauge	?	San Diego	US	?	?	
325	Zoppot		Wilke	Wilke	?		PL	?	?	

## Register of existing modern Sixes - in order of boat name - public version - v. 10 of 22nd March 2008

	Most recent known name of the boat	Sail No.	Hull Code	Launch year	Architect short name	Builder short name	Status	Home Port	Home Country	Owner	
										Name	e-mail
1	Ah Si Si	SWE 110	1979US03		Mull	Goetz	sr		SE		
2	Arunga	USA 118	1982SE03	1981	Pettersson, P	Båtbyggarna	sr	Puget Sound	US		
3	Astree	FRA 69	1969DE01	1969	Lehmann	Lehmann	sr	Noirmoutier	FR		
4	Battlecry VI	GBR 89	1988UK01	1988	Howlett	Webb	sr	Hamble	UK		
5	Bear	GBR 102	1988SE01	1988	Pettersson, P		sr	Hamble	UK		
6	Beep-Beep	SUI 108	1987CH01	1987			sr	Geneva	CH		?
7	Berta	SWE 121	1988SE02	1988	Norlin	J&Z	sr		SE		?
8	Biwi Magic	K 92	1989UK01	1989	BADT	Pencarrow	sr			?	?
9	Blade	SWE 127	1987UK01	1987	Howlett	Elephant	sr		SE		
10	Bluebird	SWE 124	1982SE04	1989	Pettersson, P	J&Z	sr		UK		
11	Bravade VI	FRA 116	1988SE05	1988	Norlin	J&Z/F	sr	St. Tropez	SE		
12	Capriccio	USA 122	1985US01	1985	Kirby	Jespersen	sr	Larchmont	FR		
13	Carina	SWE 120	1988SE03	1988	Pettersson, P	Båtbyggarna	sr		US		
14	Chisca	SWE 128	1987FI01	1987	W&R	Still	sr		SE		?
15	Conch Fritters	BAH 1	1982SE01	1988	Norlin	Carlssons	sr	Hamble	UK		
16	Courage III	GER 49	1972AT01	1969	Weber, O	Biatel	ow	Bodensee	DE		
17	Courage IX	GER 118	1989SE06	1988	Pettersson, P	Båtbyggarna	sr		DE		
18	Cream	GBR 100	1979SE02	1979	Pettersson, P	Båtbyggarna	sr		UK		
19	Delphine	SWE 125	1989SE02	1989	Pettersson, P	Båtbyggarna	sr	KSSS	SE		
20	Discovery	USA 109	1978US02	1978	Peterson, D	New Orleans	sr	Newport Beach	US		
21	Duclop	SUI 100	1981SE08				sr	Mies	CH		
22	DV Pacific Highway	GER 10	1982AU01	1982	D&L	Barnett	sr	Germany	UK		?
23	Etoile du Midi	FRA 94	1978SE01	1978	Norlin	Spillersboda	sr	St. Tropez	FR		
24	Evelina	SWE 134	1995SE01	1995	Norlin	Palm	sr	Stockholm	SE		
25	Fastasch	SWE 83	1976SE02	1976	Holm, T	Bälsta	ow			?	?
26	Filippa	SWE 130	1981SE12	1981	Pettersson, P	Båtbyggarna	sr		DE		?
27	Finnegan	USA 123	1985US02	1985	Wertheimer	Martin	sr	Seattle	US		



	Most recent known name of the boat	Sail No.	Hull Code	Launch year	Architect short name	Builder short name	Status	Home Port	Home Country	Owner	
28	Fraganita	SWE 84	1976SE03	1976	Holm, T	Stockholm B	ow			?	?
29	Frenzy	USA 107	1978US01	1978	Chance	Roberts	sr		US		
30	Georgia	GBR 99	1990UK01	1990	Howlett	Elephant	sr	Hamble	UK		
								Bellagio, Lake			
31	Gitana Jr	Z 107	1986CH01	1986	Grobéty	Durr	sr	Como	IT		
32	Gitana Sr	SUI 78	1982CH01	1982	Stephens	Egger	sr	Geneva	CH		?
33	Goodwood	NED 20	1971DE01	1971	Lehmann	Lehmann	sr	Antwerp	NL		
34	Gosling	GER 86	1971UK01	1971	Stephens	McGruer & Son	sr	Bodensee	DE		?
					Pettersson, P	Båtbyggarna					
35	Gubbröra	SWE 135	1985SE04	1985			sr		SE		
36	Halv Tolv	SWE 95	1978SE03	1978	Mull	Stenungsgrund	ow			?	?
37	Heterosex	SWE 98	1980SE01	1980	Wallin		ow			?	?
38	Holmgang	SWE 111	1983SE02	1981	Norlin	Spillersboda	sr	Slevikkilen	NO	?	?
39	Irene	SUI 91	1977NO01	1977	Pettersson, P	Abrahamsen, B	sr	Geneva	CH		
40	Jane Ann	USA 105	1983SE01	1983	Pettersson, P	Båtbyggarna	sr	St. Tropez	MN		
41	Jeu de Vent	SUI 72	1981SE11	1980	Pettersson, P		sr	Lake Constance			
42	Jungfrun	SWE 112	1986SE02	1986	Norlin	J&Z	sr		SE		
43	Junie	DEN 64	2002DK01	2002	Jensen, W	Jensen, J	sr	Denmark	DK		
44	Junior	SUI 77	1981SE10	1980	Pettersson, P	Båtbyggarna	sr	Geneva	FR		
45	Kirlo	GBR 81	1981UK01	1981	Howlett	Armstrong	sr	England	UK		
46	Kontrapunkt V	GER 11	2000DE01	2000	Hempel		sr	Fredrikshaven	DE		
47	La Difference	SUI 60	1975AU01	1975	D&L	Barnett	sr	Romanshorn	CH		
48	Lion	GBR 90	1988UK02	1988	Jones	Webb	sr	Hamble	UK		
49	Manuae	FRA 118	1977SE01	1977	Pettersson, P	Båtbyggarna	sr	Brest	FR		
50	Marilyn	SWE 101	1981SE05	1981	Norlin	J&Z	sr		SE		
51	May Be XIV	SWE 115	1987SE02	1987	Pettersson, P	Båtbyggarna	ow		SE	?	?
52	Michel	GER 20	1969DE02	1969	Lehmann	Lehmann	sr		GE		?
53	Michel V	GER 52	1986AT01	1986	J&V	Biatell	sr	Bodensee	DE		?
54	Miss Crocodile	FRA 114	1985SE01	1985	Pettersson, P	Båtbyggarna	sr	La Reunion	FR		
								Salmsach,			
55	Moby Dick	SUI 73	1978SE06	1978	Norlin	Jonsson	sr	Bodensee	DE		
56	New Sweden	SWE 129	1989SE03	1989	Norlin	Bohuslanska	sr		UK		
57	Nisida	SUI 106	1981SE09	1982	Norlin	J&Z	sr	Stachen	CH		?
58	Nivola	ITA 74	1984SE01	1984	Pettersson, P	Båtbyggarna	sr	Milan	IT		?
59	Notorious	SWE 114	1987SE04	1987	Norlin	J&Z	sr		SE		

	Most recent known name of the boat	Sail No.	Hull Code	Launch year	Architect short name	Builder short name	Status	Home Port	Home Country	Owner	
60	Ondée	Z 59	1977AT01		Mull	Biatel	sr	Geneva	CH		
61	Pacemaker	KA 8	1972AU01	1973	Stephens	Halvorsen	sr	Puget Sound	US		?
62	Prince Alfred II	KA 12	1982AU02	1982	Mull	Falshaw	sr	Sydney	AU		
63	Ranger	USA 112	1979US02	1978	Mull	Goetz	sr	Maine	US		?
64	Razzle-Dazzle	GBR 82	1975US02	1975	Peterson, D	Eichenlaub	sr	Fowey	UK		
65	Rebecca	SWE 119	1989SE07	1988	Pettersson, P	Privat	sr		SE		
66	Rumble Bee	SWE 99	1981SE06	1981	Norlin	Heed	sr		UK		
67	Scallywag	GBR 97	1979US01	1979	Mull	Eichenlaub	sr	Cowes			?
68	Scoundrel	GER 96	1986UK01	1989	Howlett	Litton	sr	Romershorn	UK		
69	Scoundrel I	GBR 86	1986SE01	1986	Pettersson, P	Båtbyggarna	sr	Plymouth	UK		?
70	Shadow VI	GBR 93	1988UK03	1988/98	BADT / Hollom	Pencarrow / Farman	sr	Hamble	UK		?
71	Socketeye	USA 119	1978SE05	1978	Norlin	Jonsson	sr	Puget Sound	US		
72	Sophie II	SWE 132	1991SE01	1989/94	Norlin	J&Z	ow		SE		?
73	St. Francis V	USA 100	1973US01	1973	Mull	Stone	sr	Newport Harbour	US		
74	St. Francis VI	NOR 89	1975US04	1975	Mull	Lee	sr	Lymington	UK		
75	St. Francis VII	USA 108	1979US04	1978	Mull	Lee	sr	Larchmont	US		
76	Sting	SWE 123	1989SE05	1989	Pettersson, P	Båtbyggarna	sr		SE		
77	Suncraft	SWE 76	1975FI01	1975	Sundén	Sarin	ow			?	?
78	Suncraft II	SWE 81	1976SE01	1976	Sundén	Jacobson	ow			?	?
79	Tazio	ITA 75	1987UK02	1987	Howlett	Elephant	sr		IT		?
80	Tempest	GBR 105	1986US01	1985	S & S	Goetz	sr	Hamble	UK		
81	Temptation II	SUI 113	1984UK02	1982	Howlett		sr	Lac Thun	CH		
82	Temptation III	SUI 104	1982SE02	1981	Pettersson, P	Båtbyggarna	sr	Lac Thun	CH		
83	Thisbe	HKG 2	1987SE01	1987	Pettersson, P		sr	Lymington	UK		
84	Toogooloowoo IV	KA 6	1967AU01	1967	Stephens	Barnett	sr	Berlin	DE		
85	Toogooloowoo V	FIN 55	1970AU01	1970	Stephens	Barnett	sr	Finland	FI		
86	Tsigane	FRA 107	1982UK02	1982	Howlett	Thompson	sr	Trebeurden	FR		
87	Valantina	ITA 70	1977IT01	1977	Mull	Sangermani	sr	Genoa	IT		
88	Valhalla	GBR 107	1981SE07	1981	Norlin	J&Z	sr		UK		
89	Warhorse	FRA 106	1977US01	1978	Wertheimer	Cadranell	sr	Rennes	FR		
90	Wildcat II	FIN 62	1989KN01	1987	Howlett	Chivers	sr	Finland	UK		
91	Winchala	SUI 71	1975US03				sr	Carouge	CH		
92	Woodoo	SUI 122	1990SE01	1989	B&R	Saltsjöbaden	sr	Lausanne	CH		

